



Batemans Bay
Master Plan

Eurobodalla
Shire Council

Draft

Acknowledgment of Country

Eurobodalla Shire Council recognises Aboriginal people as the original inhabitants and custodians of all land and water in the Eurobodalla and respects their enduring cultural and spiritual connection to it. Eurobodalla Shire Council acknowledges the Traditional Owners of the land in which we live. Council pays respect to Elders past, present and aspiring. We are on Yuin Country.

How to contact us

- In person** Customer Service Centre
Corner Vulcan and Campbell Streets, Moruya
Monday to Friday, 8.30am to 4.30pm
- Phone** 02 4474 1000
For after-hours emergencies call 1800 755 760
- Mail** PO Box 99, Moruya NSW 2537
- Email** council@esc.nsw.gov.au
- Web** www.esc.nsw.gov.au
- Councillors** See contact details on our website

Cover credit: Eurobodalla Coast Tourism

Table of Contents

Table of Contents	2
Figures	5
Tables	7
Executive Summary	8
Aboriginal cultural and heritage.....	8
Why are we Doing this?.....	10
Aims of Plan	12
Key Policy Areas	12
Vision Statement.....	14
Themes	17
Theme One: Flourishing.....	18
Definition:.....	18
Town Character:.....	18
Future Condition:	18
Planning Objectives:.....	18
Planning Principles:	18
Theme Two: Morphology	20
Definition:.....	20
Future Condition:	20
Planning Objectives:.....	20
Planning Principles:	20
Theme Three: Typology	22
Definition:.....	22
Town Character:.....	22
Future Condition:	22
Planning Objectives:.....	22
Planning Principles:	22
Theme Four: Ecology	24
Definition:.....	24
Town Character:.....	24
Planning Objectives:.....	24
Planning Principles:	24
Theme Five: Capacity.....	26

Definition:.....	26
Town Character:.....	26
Future condition.....	26
Planning Objectives.....	26
Planning Principles.....	26
Primary Localities.....	29
1 Tourism Quarter.....	30
Challenges.....	33
Positive outlook.....	34
Key sites.....	34
1 Smoke Point.....	34
2 Clyde Street.....	37
3 West Vesper Street.....	38
4 Perry Street.....	40
5 North Orient Street.....	41
6 Murra Murra Mia Walkway.....	42
2 Commercial Quarter.....	44
Overview.....	44
Challenges.....	46
Positive outlook.....	47
Key sites.....	48
1 Highway layover and Bus stop.....	48
2 South Orient Street.....	49
3 Soldiers Club.....	50
4 Camp Street.....	51
5 The Water Gardens.....	52
6 Old Princes Highway.....	53
3 Residential Quarter.....	54
Overview.....	54
Challenges.....	55
Positive outlook.....	55
Key sites.....	55
1 Bent Street.....	55
2 Beach Road.....	56
3 Bavarde Avenue.....	57
4 Batemans Bay Marina.....	58
5 Batemans Bay Marina Resort.....	59
6 Catalina Golf Club car park and Golf Links Drive.....	61
7 NSW Marine Rescue Facilities.....	62

8 Hanging Rock Sports Fields	63
9 Batehaven	64
4 Industrial Quarter	65
Overview	65
Challenges	67
Positive outlook.....	67
Key sites.....	68
1 Western Industrial.....	68
2 Eastern Industrial Area.....	69
Actions	70
1 Tourism Quarter	70
2 Commercial Quarter	73
3 Residential Quarter	76
4 Industrial Quarter	78
Policy Actions.....	79
Appendices	80
Appendix 1 – The Master Plan	81
Appendix 2 – Key sites ELEP 2012 height map changes	82
Appendix 3 – Proposed Policy Changes	89
Policy Changes.....	89
Heritage and Indigenous Identity	89
Streets and Laneways.....	89
Greening.....	90
Street trees.....	90
Celebratory Spaces.....	91
Outdoor Refreshment Areas	91
Communal Pavilions.....	91
Managing form.....	91
Design Excellence.....	93
Urban Form and Fabric Incentives	94
Natural Forms.....	94
General design policy.....	94
Four incentives to be implemented in this plan:	95
Building and Mass	95
Articulation.....	95
Outdoor Dining Areas	96
Sky Terraces.....	96
Land Zone and Land Use Table changes	97

Figures

Figure 1 - Welcome to Country.....	9
Figure 2 Batemans Bay Town Centre.....	10
Figure 3 The Study Area.....	11
Figure 4 Indicative Tourism and Commercial Quarters Opportunity sites.....	15
Figure 5 Indicative Gateway and Landmark sites.....	15
Figure 6– Opportunity Sites.....	16
Figure 7 Aerial View from southwest of town centre.....	16
Figure 8 Vision for the Future.....	17
Figure 9 Example of a temporary food van.....	19
Figure 10 Spatial Plan.....	21
Figure 11 Example Outdoor Dining.....	23
Figure 12 Example of connection of public and private realms.....	23
Figure 13 Examples of water garden formal design features.....	25
Figure 14 Changing hierarchy of movement.....	27
Figure 15 Example of street tree fairy lighting.....	28
Figure 16 Four Quarters.....	29
Figure 17 Tourism Quarter.....	30
Figure 18 Master Plan – Tourism Quarter.....	31
Figure 19 Jetty with boat docking.....	32
Figure 20 Tourism Gateway and Landmark sites.....	33
Figure 21 Smoke Point.....	34
Figure 22 Example of waterside dining with docks.....	35
Figure 23 Examples of adaptable reuse of vessels as a floating restaurant.....	36
Figure 24 Clyde Street.....	37
Figure 25 West Vesper Street.....	38
Figure 26 – Mangrove Boardwalk.....	39
Figure 27 Perry Street.....	40
Figure 28 Perry Street urban green space concept.....	41
Figure 32 North Orient Street.....	41
Figure 29 Murra Murra Mia Walkway.....	42
Figure 30 Murra Murra Mia Walkway form, shadow and massing.....	42
Figure 31 Historic Photo of Natural Swimming Pool at Batemans Bay.....	43
Figure 33 Commercial Quarter.....	44
Figure 34 Master Plan – Commercial Quarter.....	45
Figure 35 Highway layover and Bus Stop.....	48
Figure 36 South Orient Street.....	49
Figure 37 Examples of angle parking suitable for Orient Street south, Flora Crescent.....	49
Figure 38 Soldiers Club.....	50
Figure 39 Camp Street.....	51
Figure 40 The Water Gardens.....	52

Figure 41 Old Princes Highway	53
Figure 42 Old Princes Highway transition from residential to mixed-use.....	53
Figure 43 Residential Quarter	54
Figure 44 Bent Street	55
Figure 45 Possible built form along Beach Road	56
Figure 46 Bavarde Avenue	57
Figure 47 Batemans Bay Marina	58
Figure 48 Batemans Bay Marina Resort	59
Figure 49 Potential height and buildings at the Marina Site	60
Figure 50 Catalina Golf Club and Golf Links Drive	61
Figure 51 NSW Marine Rescue Facilities.....	62
Figure 52 Hanging Rock Sports Fields	63
Figure 53 Batehaven	64
Figure 54 Industrial Quarter	65
Figure 55 – Master Plan – Industrial Quarter	66
Figure 56 Western Industrial Area.....	68
Figure 57 Eastern Industrial Area	69
Figure 58 The Tourism opportunities	70
Figure 59 The Commercial opportunities	73
Figure 60 Residential opportunities.....	76
Figure 61 Industrial Quarter action sites	78
Figure 62 Combined Height Map depicting proposed changes to existing mapping.....	83
Figure 63 Map of area depicting proposed RL1 – 0 to 20m	84
Figure 64 Map of area depicting proposed RL2 – 20 to 40m	85
Figure 65 Map of area depicting proposed RL3 – 40 to 60m	86
Figure 66 Map of area depicting proposed RL4 – 60 to 80m	87
Figure 67 Map of area depicting proposed RL5 – 80 to 100m	88
Figure 68 Indicative centreline and boulevard style tree planting and car parking options...	90
Figure 69 Wave Form Approach to the skyline	92
Figure 70 Amphitheatre style of construction heights	93
Figure 71 Façade articulations are allowed to project into the building setback	96
Figure 72 The sky terrace.....	96
Figure 73 Current Land zone map in ELEP 2012	97
Figure 74 Tourism Quarter current and future zoning	98
Figure 75 Current and future zoning	98
Figure 76 Marina Zone changes.....	99
Figure 77 Example of the composition heights as a built-form outcome	100
Figure 78 LEP2012 standard instrument heights.....	100
Figure 79 Current and future heights Tourism Quarter	101
Figure 80 Current and future height Commercial Quarter	102
Figure 81 Height changes Bent Street	103
Figure 82 Height changes Bavarde Street.....	103
Figure 83 Height changes Batemans Bay Marina Resort.....	104
Figure 84 Height changes Catalina Golf Course.....	104

Tables

Table 1 Tourism opportunity sites	72
Table 2 Commercial opportunities sites	75
Table 3 Residential opportunities	77
Table 4 Industrial Opportunities	78
Table 5 Policy Opportunities	79
Table 6 Height variation incentives for movement within each m(RL) 20m band	101

DRAFT

Executive Summary

Batemans Bay is a beautiful holiday destination where Clyde River and Pacific Ocean meet. The Eurobodalla vision aims to create a vibrant, connected community with new developments like the Clyde Bridge and Bay Pavilions Aquatic and Arts Centre.

By 2100, the goal is to blend natural beauty with an urban environment, meeting the communities' needs and addressing climate change.

The plan focuses on enhancing employment, public spaces, and economic sustainability while respecting the town's pace of life, environment, and heritage.

The Council works with NSW government agencies to create safe, vibrant places, emphasising sustainability and balancing urban growth with environmental sensitivity. The plan promotes sustainable tourism and sets growth boundaries, ensuring development aligns with community-defined beauty.

Aboriginal cultural and heritage

The people of the Yuin Nation are the traditional custodians of the land we now know as Eurobodalla Shire Local Government Area. Yuin people have lived in the area for thousands of years and have an enduring custodianship and connection to the land and waterways.

The local Walbunja community in Batemans Bay actively raises awareness and respect for protecting significant cultural resources. The history, both Aboriginal and European, establishes a unique identity and ties cultural and aesthetic experiences. This is crucial for place-making, creating identity, and tourism, which supports economic sustainability and guides the town's growth.



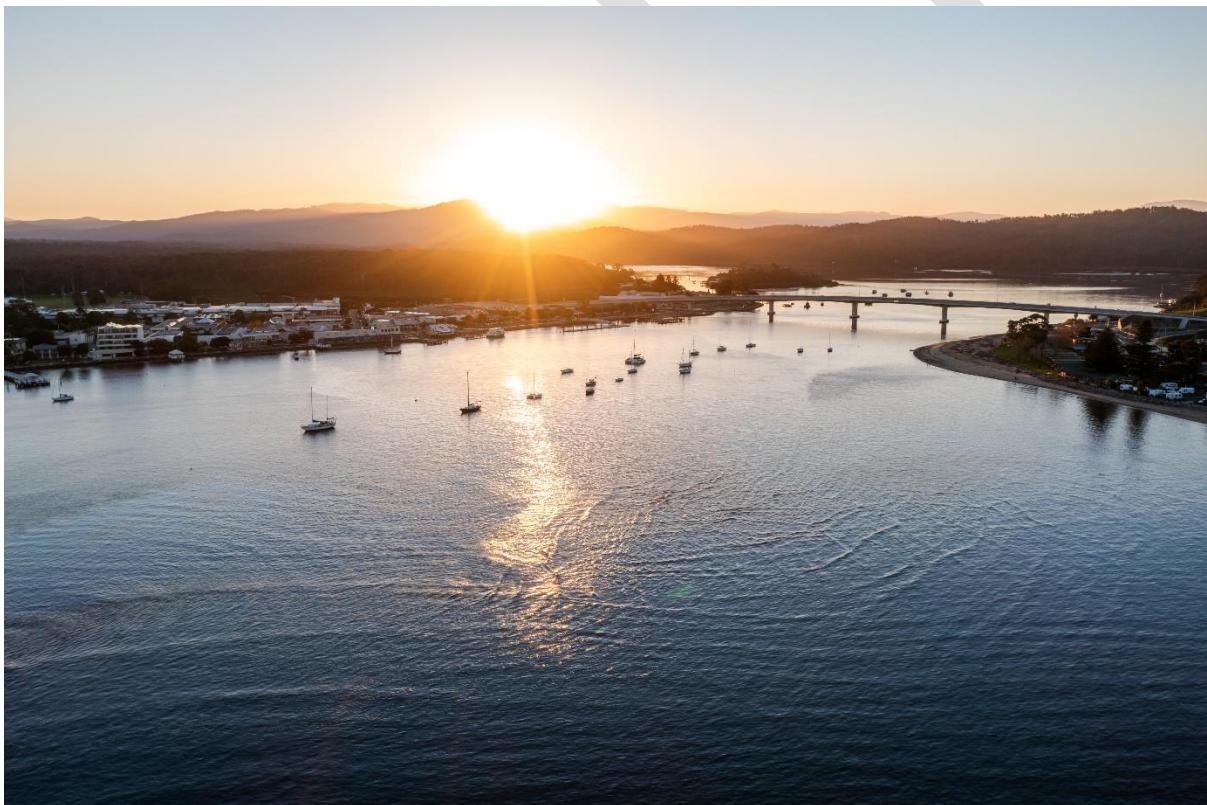
*Figure 1 - Welcome to Country
Credit: Eurobodalla Coast Tourism*

Why are we Doing this?

Batemans Bay is divided by the Clyde River into North Batemans Bay (including Surfside) with the southern side containing the town centre and suburbs of Catalina, Batehaven, Denhams Beach, and Surf Beach. The town features a beautiful coastal and riverine landscape with stunning water and mountain views, lush tree cover, and large open spaces. Its linear urban footprint covers 2,033ha and offers ideal 10-20 minute walkable neighbourhoods.

This plan aspires to create a beautiful, vibrant, sustainable and inclusive town which provides for future generations and enables the key significant features of the locality are improved and preserved for future generations. While not prescribing individual designs, this plan highlights preferred key sites, growth areas and beautiful places. The plan applies to the area as depicted in Figure 3.

This plan aims to guide the town's growth to 40,000 residents by 2100. This natural population growth will vary depending on economic trends.



*Figure 2 Batemans Bay Town Centre
Credit: Eurobodalla Coast Tourism*



Figure 3 The Study Area

Aims of Plan

The plan aims to:

- To create a beautiful, functional and thriving town centre.
- Define the boundaries of the Tourism, Commercial, Residential and Industrial quarters.
- Identify the key areas for growth for the quarters within the town.
- Reinforce the greenbelt and limits of urban expansion along the Coast.
- Transform the town from a day based to a night and day-based economy.
- Encourage the 15 to 25 age group to remain in the town as part of a tourism, hospitality and entertainment sector through social and economic incentives.
- Increase the diversity in range, styles and types of housing to meet changing demographics and tourism needs.
- Transition the town centre from low-density commercial developments to mixed-use higher-density development.

Key Policy Areas

1. Redefining Spatial Arrangement:

- The plan will redefine the spatial arrangement of the town's functions, developing four distinct localities with associated uses:
 - Tourism Quarter: A mixed-use area facilitating living, working, playing, visiting, shopping and entertainment.
 - Residential Quarter: Offers a wider range of living options for an aging population, tourism and conferencing facilities, and localised retail opportunities.
 - Commercial Quarter: The Commercial Quarter needs reinforcement; it will be redefined to facilitate significant floorplate commercial activities and mixed-use residential accommodation.
 - Industrial Quarter: Will find new opportunities to expand and redevelop.

2. Movement and Transportation

- The plan proposes Council revisit the movement of people and vehicles to create quieter areas, develop more pedestrian and bike-focused facilities, and transform some roads into public spaces, tree-lined boulevards and shared spaces.

3. Increasing Residential Density

- The plan focuses on increasing residential density through two combined options:
 - Smaller lot sizes: More townhouses and terrace accommodations.
 - Additional height: Introducing height in selected locations within the tourism quarter and adjusting maximum height provisions in some residential areas.

4. Creating Enjoyable Spaces:

- Enhancing open spaces, increasing tree canopy and incorporating aesthetic considerations into development outcomes to create enjoyable spaces. Examples include changes to Perry Street and the Water Gardens to create new alternative spaces.

5. **Environmentally Sensitive Infrastructure Management:**

- Managing infrastructure in an environmentally sensitive way is a fundamental driver, including:
 - Stormwater Management: Addressing underdeveloped stormwater facilities and improving water management across all localities.
 - Other Infrastructure: Reconfiguring car parking, bus routes, waste management facilities, public toilets and open space infrastructure, including play and recreation areas.

DRAFT

Vision Statement

Shaping a vibrant future for Batemans Bay

Plans must be adaptable, especially in the face of change. This plan considers the limitations of the current town and plans the most appropriate way to increase density in planned locations.

Council aims to achieve additional housing by nominating key sites across the study area which are suitable for increased height.

This plan aims to embrace both the natural advantages of the bay and the opportunity to improve the beauty and experience of its urban environment while identifying suitable development opportunities for future generations.

The vision for the Bay focuses on creating a dynamic and appealing town and exploring opportunities for diversification. It balances resident and visitor well-being that connect between people and places while supporting healthy environments and the importance of art and culture. The plan emphasises the town becoming an attractive and vibrant destination. This involves creative place making, culinary experiences, tourism and recreational activities along with providing for commercial and housing growth.

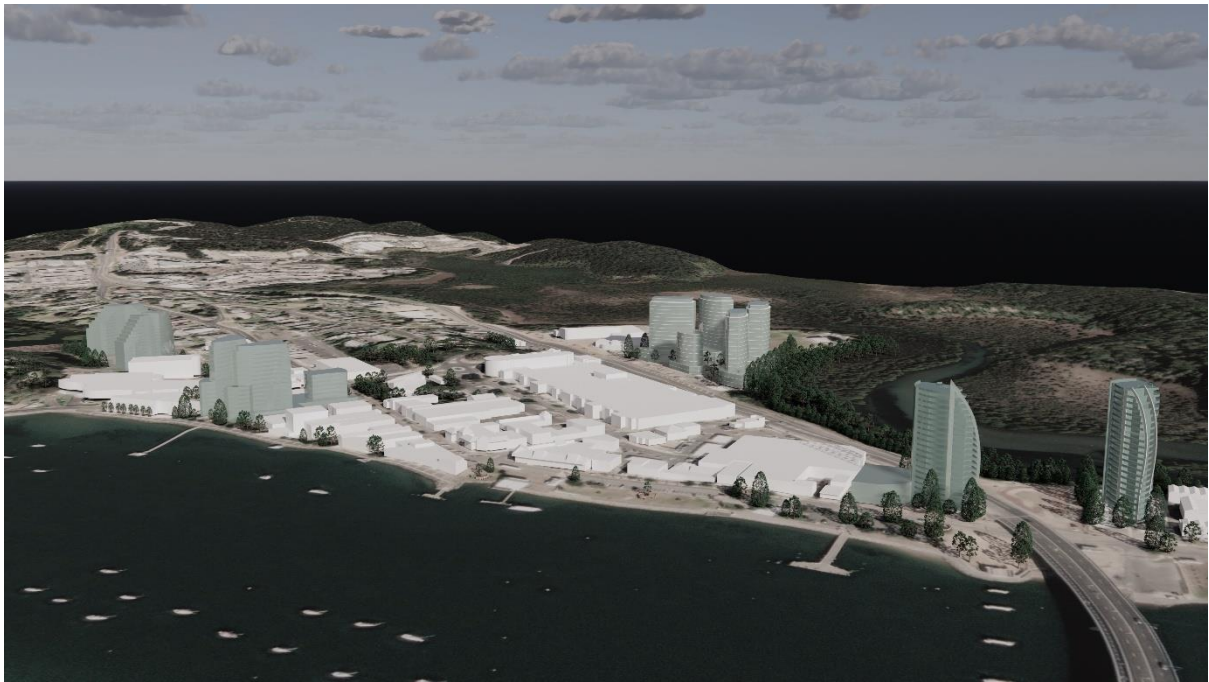


Figure 4 The future scale of town centre growth by 2100



Figure 5 Five key Gateway and Landmark sites



Figure 6– Aerial view from the northeast of the study area



Figure 7 Aerial View from southwest of town

Themes

The primary attributes of the town are organised under five themes. This approach helps to break down the issues, objectives and actions into manageable frameworks and arrange ideas logically. By categorising the attributes into themes, tackling complex problems systematically and developing clear, actionable plans becomes easier. This method provides structure and clarity, making it more straightforward to identify priorities and implement solutions effectively.

The themes inform the actions applying to each quarter and policy action for the Council.



Figure 8 Vision for the Future

Theme One: Flourishing

Definition: Flourishing encompasses a thriving community where residents and visitors benefit from economic growth opportunities, enhanced infrastructure, and a wider range of amenities and services of a beautiful place.

Town Character: Batemans Bay, the largest settlement in the Eurobodalla region, is rapidly approaching its urban growth limits due to the natural boundaries like rivers, the ocean, hills, wetlands, and forests. The town's core has shifted over time, with commercial activity migrating north of Beach Road, leading to regeneration and redevelopment.

Future Condition: Batemans Bay will flourish as people move into well-located and connected social, economic and natural environments that provide for human health and wellbeing. Restoring character to the town centre will add variety to the urban fabric, creating visual contrast and rejuvenating dilapidated areas. The town will celebrate its heritage, integrating new buildings sensitively into the existing landscape.

The town centre and adjoining residential areas will allow for intensification of development without impacting on the local surrounding environment.

A vibrant economy will provide active and open spaces, supporting a thriving day and night economy.

Planning Objectives:

1. Foster creative and nighttime economic use for niche and artisan retail opportunities.
2. Enhance design appeal by transitioning design quality based on creating beautiful places.
3. Focus on quality of life, recognising the health benefits of beautifully designed spaces.
4. Highlight the connection of place to food and local food traditions.
5. Apply a tactical public realm approach to creating main streets as living rooms, canvasses, and experimental spaces that identify, celebrate, and leverage placemaking opportunities in our street network.

Planning Principles:

1. Reinforce of the identity of the 'main street', food market, foreshore boardwalk, floating village, commercial business district, town park, marina residential area, hanging rock sports and Batehaven as destinations.
2. Consolidate service and facilities, eateries, office space, and new forms of residential housing and recreation spaces and experiences.
3. Support youth-based active engagements in the urban fabric, laneways and recreational spaces of the Commercial and Tourism quarters.
4. Enable opportunities for new retail and nighttime entertainment and residential typologies.
5. Promote indigenous cultural significance in design.



*Figure 9 Example of a temporary food van
Credit: Eurobodalla Coast Tourism*

Theme Two: Morphology

Definition: Morphology refers to the spatial arrangement and function of the town. It refers to how we organise the town.

Town Character: Batemans Bay features clearly defined spaces divided by significant transport corridors. The town has distinct residential and mixed-use zones catering to residents, commercial needs, and tourism. These activities are situated along the foreshore and Clyde River, blending commercial and recreational environments. South of the town centre, you'll find industrial activities, the marina, and the local centre of Batehaven. Due to the absence of a resident population in the town centre, trading is limited to daytime and business hours only.

Future Condition: Batemans Bay is in a position where significant change needs to occur to realise the full potential of the locality. The introduction of commercial office and residential accommodation will convert the town from a commercial centre to a mixed-use centre. The residential quarter offers new opportunities for a diversity of housing focused on water views and access.

Recreational areas within town will adapt over time to the increasing urban growth and need for a diversity of experiences with nature while supporting the natural environment. Utilising the natural features of Batemans Bay whilst increasing economic development will sustain the towns growth and improve the quality of life.

The focus of the morphology theme is to utilise Beach Road to separate the existing town centre into two distinct quarters and reflect the different objectives and purposes of tourism and commercial office activity. The residential quarter offers a diversity of housing opportunities.

To achieve future economic growth and respond to population demands, the review of building heights, architectural styles, zoning and uses anticipates positive change.

Planning Objectives:

1. Prioritise the town's functional and spatial layout to integrate relevant land uses, formal recreation spaces, and taller buildings into a cohesive design.
2. Redefine the town's commercial and tourism areas as individual localities and functions.
3. Strengthen, enhance, and clarify the physical structure of each quarter to provide a memorable, easily legible, and attractive town.
4. Strengthen the grid pattern street network for the CBD, and create opportunities for new road connections, better pedestrian pathways, and safe pedestrian movement links.

Planning Principles:

1. Enable the built environment to respond to natural hazards.
2. Provide greater clarity and legibility of the town structure, visually prominent sites and locations, and functions by establishing and reinforcing a clear hierarchy of gateway entrances and landmark sites, movement networks (roads, pathways, and shared spaces), and recreational spaces.

3. Promote mixed-use development that contains active street frontages on key streets, with rear-loaded vehicular access.
4. Physically emphasise the gateways, edges, hubs, and key public ceremonial spaces through building form, lighting, and landscaping design.
5. Enable connectivity of the quarters through the installation of connected shared pathways, that create a circular connected link.

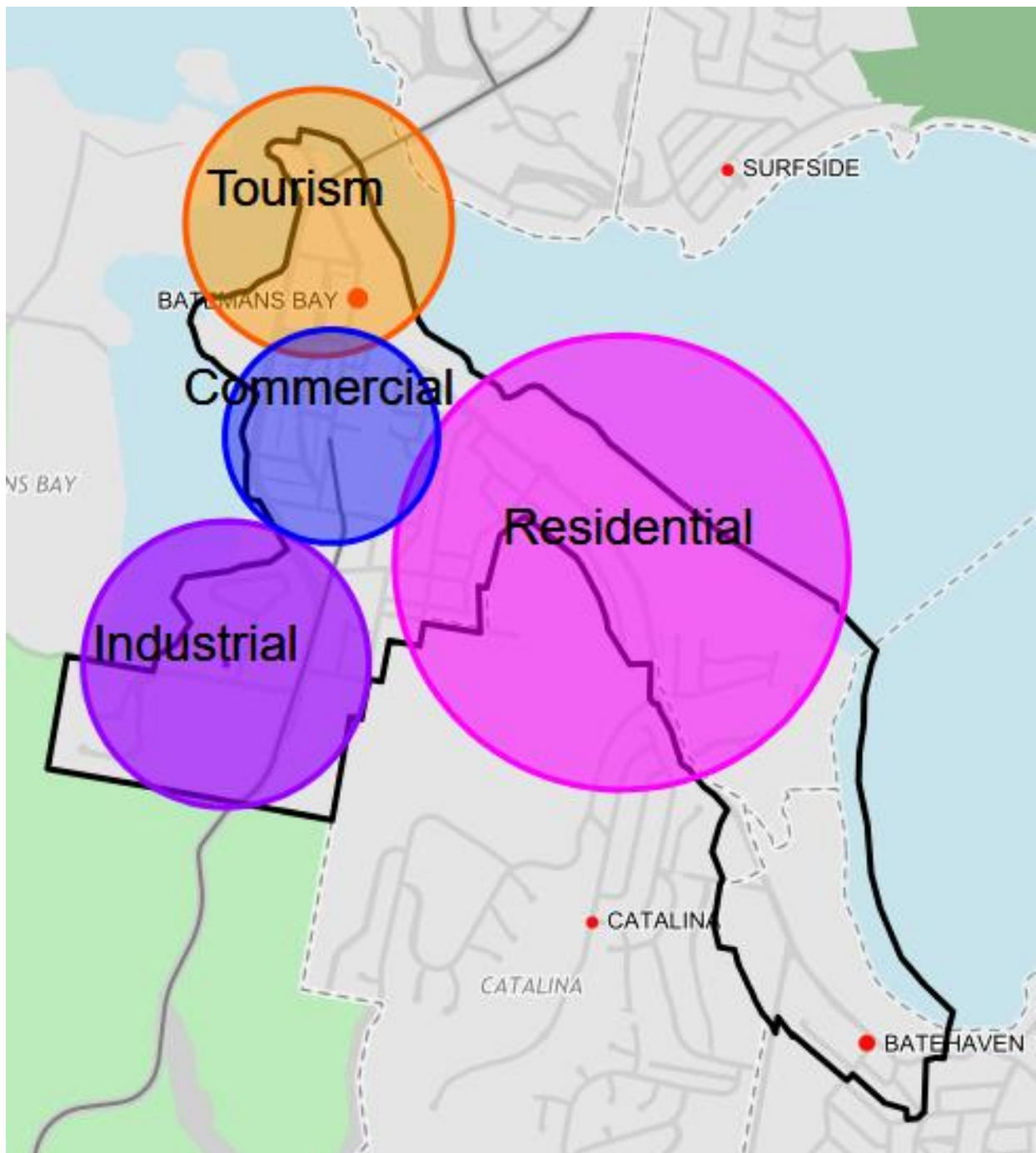


Figure 10 Spatial Plan

Theme Three: Typology

Definition: Typology refers to the characteristics and composition of buildings and places and how people interact with the buildings facing the street.

Town Character: The existing built form consists of one and two-storey developments and represents a low-density scale. The buildings have a variety of architectural styles. There has been a growing trend to convert commercial spaces to office spaces, which results in the need for privacy. This impacts how active the street frontage is. The town lacks an active nighttime economy.

Future Condition: The town requires accommodation within the commercial and tourism quarter to provide a population to establish a nighttime economy. The introduction of spaces will assist in the activation of streets and places while enhancing the natural environment of the town by activating links to these spaces.

Developing controls that relate to the activation of spaces, not only at street level, but within and on top of buildings. Developing landmark buildings in the town centre is crucial for Batemans Bay's overall growth.

Planning Objectives:

1. Require iconic architecture and urban design for key sites or prominent locations to create sculptural landmarks. These should draw from natural forms, respond to historical context and identity, and provide visually pleasing and beautiful experiences.
2. Use inspiring natural elements for distinctive design, ornamentation, and motifs for landmark and gateway buildings. These should engage the community and reflect their appreciation of beautiful architecture.
3. Highlight variations in building heights and housing types based on the quarter's function, ensuring consistency in colour, materials, symmetry, and composition.
4. Encourage mixed use development to integrate vertical green spaces with eco-friendly services and avoid rigid layouts.

Planning Principles:

1. Define Gateway and Landmark sites should offer complementary materials, colour, and lighting palettes, while each building also establishes unique sculptural features, symmetry, ornamentation, and motifs.
2. Focus on creating a form that would complement rather than compete – creating a companion building, rather than something separate and unrelated.
3. Enable building heights to increase or decrease in harmony with adjacent structures, follow height limit maps and incentives, and design policies for key site opportunities.
4. Create a deliberate and unified visual identity for key sites and public spaces within the town.
5. Activate street frontages should enhance wayfinding, enable outdoor refreshment areas, urban gardening, public street art, consistent place branding, pedestrian access, and streetscape cohesion for nearby commercial activities.



Figure 11 Example of Streetscape cohesion



Figure 12 Example of seamless transition between public and private realms

Theme Four: Ecology

Definition: Ecology focuses on open and green spaces for community enjoyment, recreation, place-making, and landscape interventions.

Town Character: Batemans Bay is where the Clyde River meets the Pacific Ocean. The town is surrounded by forested ridgelines, coastal shores, and riverine areas with bays, inlets, and oyster farms. The Water Gardens is a valued area with endangered species including a Grey Headed Flying-fox. The town's urban areas have minimal landscaping and greenery.

Future Condition: The town centre will be connected to the natural environment with shared pathways linking the parks and gardens to the natural settings of the locality.

Promoting and supporting the natural environment and its biodiversity through town growth is essential. By protecting significant natural areas and using them for educational purposes, we can raise awareness and appreciation for the local ecosystem. This approach not only supports biodiversity but also enhances nature tourism, showcasing the unique offerings of the area.

Stormwater generated by the town centre should adopt water-sensitive urban design, and these spaces become activated and diverse green spaces.

Planning Objectives:

1. Prepare a Strategy and apply greening principles to urban and built forms, including public and private areas to encourage design responses utilising natural materials and sustainably sourced products.
2. Encourage the management of the coastal and urban settlement interface with listed species by approved management plans, State policy guidance, and the EPBC Act requirements.
3. Enhance and restore biodiversity on public and private land, along wildlife corridors, and manage pest species.
4. Establish a traditional town centre park with formal landscape treatments and planting for recreation spaces supporting the CBD.

Planning Principles:

1. Utilise creative forms of landscaping to define street functionality, greening of footpaths, street corners, and pedestrian safety islands.
2. Incorporate a diversity of flora, sensory landscape and natural elements to soften road verges, buildings, and the landscaping of gateway and landmark sites and public spaces.
3. Improve the natural functioning of the Water Gardens town park and recreation opportunities whilst sympathetically maintaining the significant habitat of the area.
4. Enable the natural environment to provide a greenbelt boundary to the future growth of the town.



Figure 13 Examples of water garden formal design features

Theme Five: Capacity

Definition: Capacity assesses the networks and infrastructure, movement and transport supporting the town.

Town Character: The demand for infrastructure has changed considerably in recent years. The Old Highway moved to Vesper Street. The left turn off the old bridge into Clyde Street has been cut off and commercial activity has moved north from Orient Street to Clyde and North Streets.

Future condition

These changes open significant opportunities to change the way we use streets, provide parking and move people around. We will also create tree canopy and other landscape options and create spaces for pedestrian market and passive recreation activities.

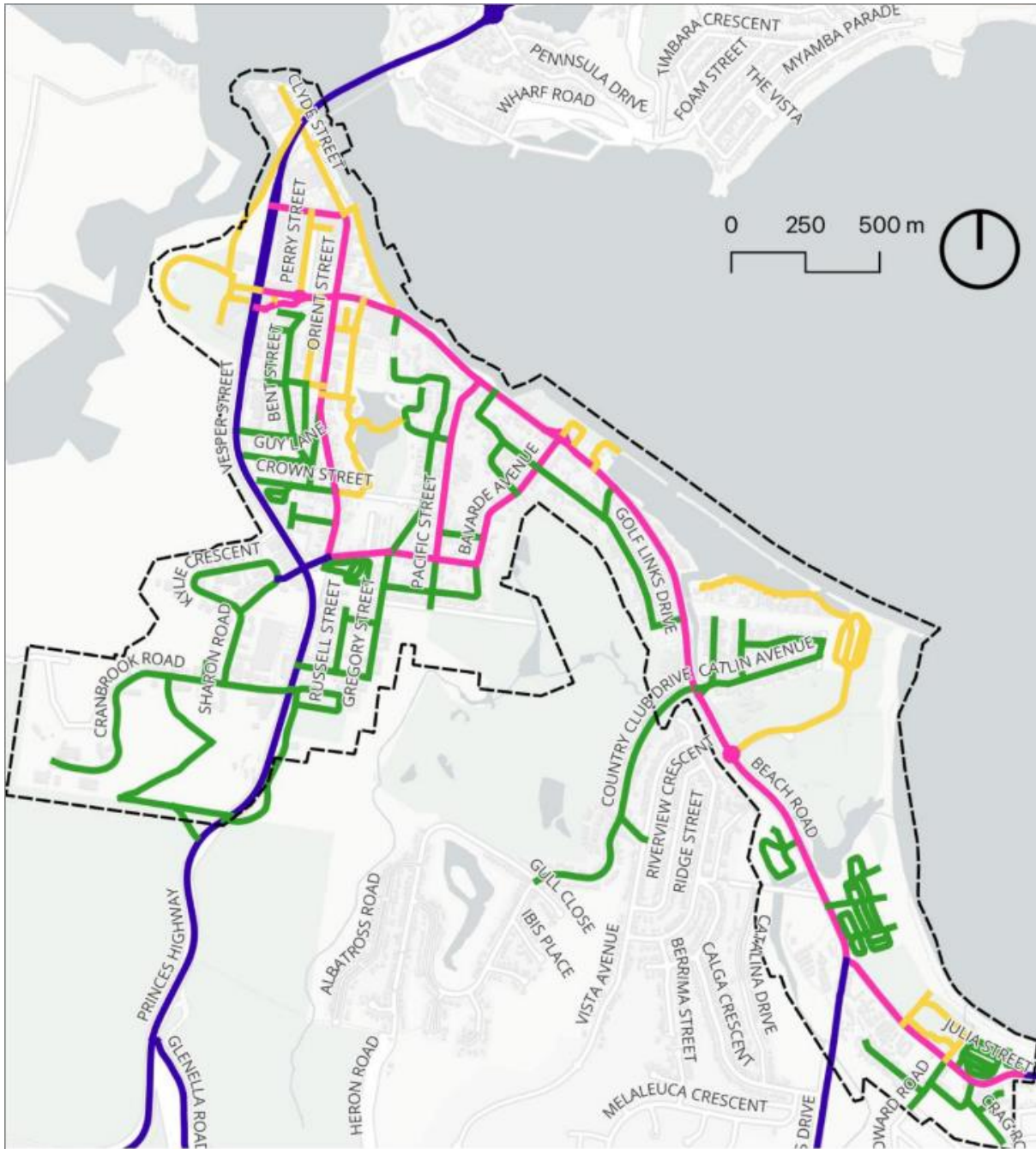
Water, stormwater and sewer infrastructure must be improved to support the expected growth.

Planning Objectives

1. Create greater permeability through transport congestion controls, pedestrian priority, and multiple connections through large blocks.
2. Promote active and public transport networks and connections through and around each town quarter with shared bike and pedestrian pathways, public transport facilities, community infrastructure, and recreational shelter options.
3. Prioritise the public realm delivery of comfortable, permeable, and safe spaces to encourage exploration and lingering.
4. Enhance the town centre southern entry to Old Princes Highway and the commercial 'main street'.

Planning Principles

1. Pedestrian pathways and shared paths are to be connected, well-lit, and interesting spaces containing public art.
2. Require street trees to define the character and hierarchy in the public realm, with a focus on pathways adjacent to active frontages and tree lighting.
3. Maintain a suitable level of service appropriate to the road hierarchy.
4. Promote the installation of charging stations in suitable locations.



LEGEND

Batemans Bay Master Plan Study Area Boundary

Network Classifications (Desired/Aspirational)

Civic Space

Local Street

Main Road

Main Street

Figure 14 Changing hierarchy of movement



Figure 15 Example of street tree fairy lighting

Primary Localities

Each locality has specific functional opportunities making up a quarter of the overall town:

- Tourism Quarter
- Commercial Quarter
- Residential Quarter
- Industrial Quarter

These areas have set boundaries to guide development, but over time, the differences between them may blur as they grow and merge. This is evidenced in the southward migration of commercial services into the residential areas along the Old Princes Highway.

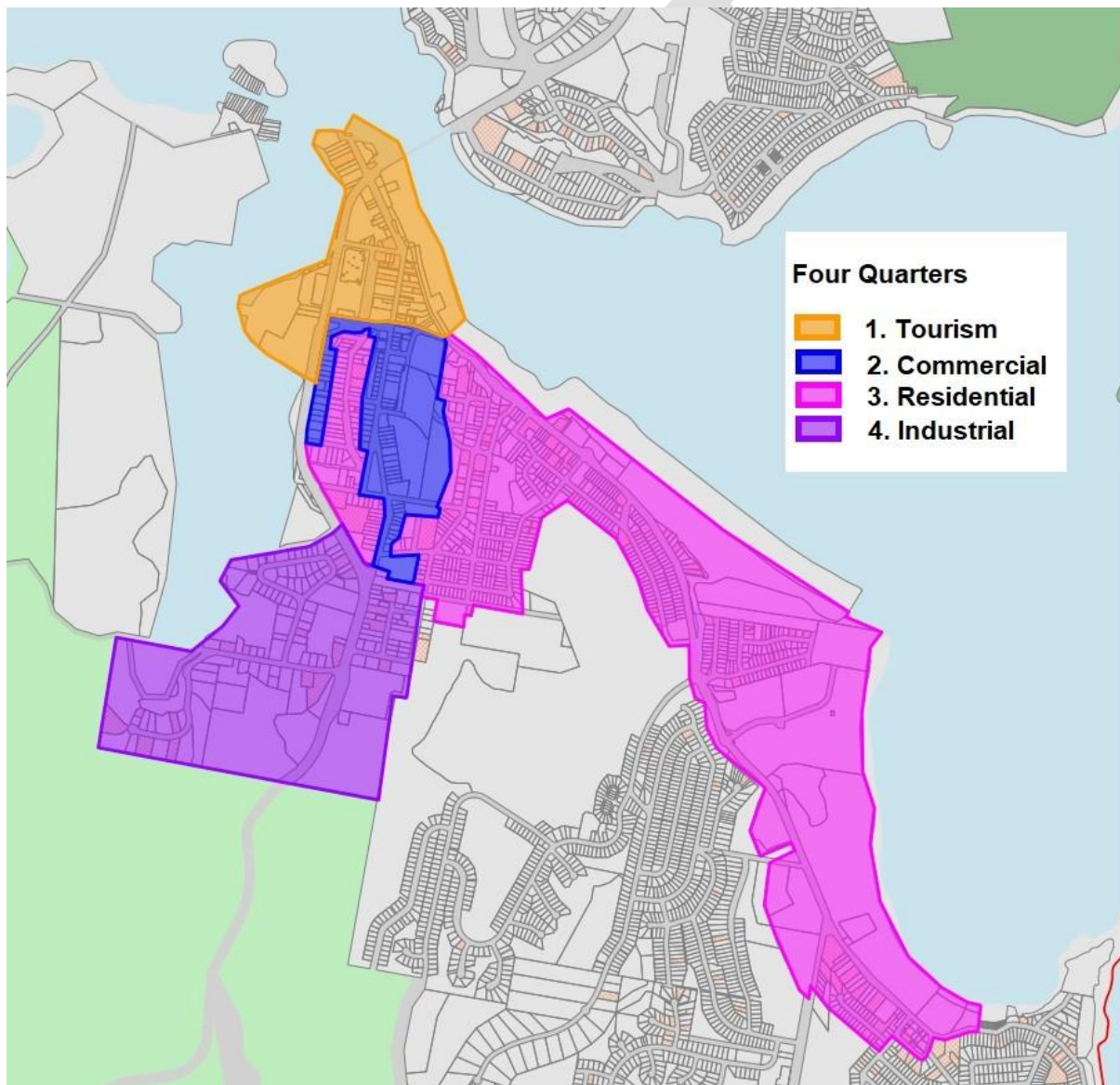


Figure 16 Four Quarters

1 Tourism Quarter



Figure 17 Tourism Quarter

Batemans Bay is the key service centre for Eurobodalla Shire. The Tourism Quarter function is dominated by retail supermarkets, but it lacks a tourism and related accommodation emphasis. The town centre also lacks a defined commercial office activity and business centre. Focus on connecting high value tourist activity with mixed commercial and residential functioning will enhance its future.

The area contains opportunities for hotels, short-term rental housing, serviced apartments, and shop-top housing to address housing needs. This will assist in establishing a nighttime economy.

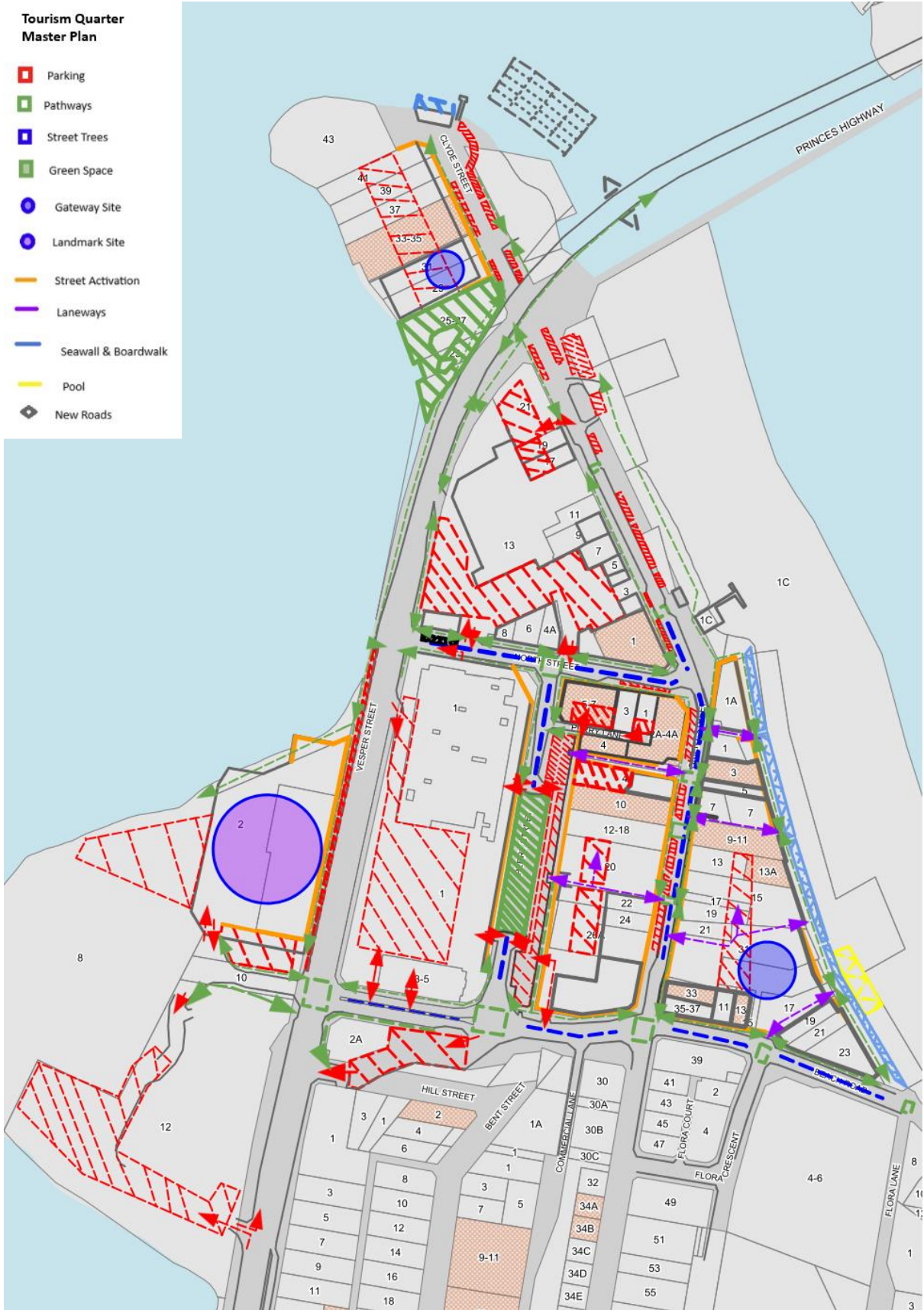


Figure 18 Master Plan – Tourism Quarter

Land uses designated for the MU1 Mixed Use and W2 Recreational Waterways zones offer a variety of development opportunities. Additionally, other temporary land and water use and site-specific developments that could be explored in this quarter include:

- Aquatic pavilion
- Natural swimming pool
- Hotels and restaurant
- Artisan Food and Drink Premises, including Microbrewery pub, Distillery centre
- Galleries
- Gateway landmarks featuring iconic, innovative, and sustainable architecture
- Enhancement of the Murra Murra Mia Walkway foreshore walkway
- Riverside mangrove shared pathway connecting Smoke Point to Mackay Park
- Houseboat mooring field and
- Floating bar and restaurant.

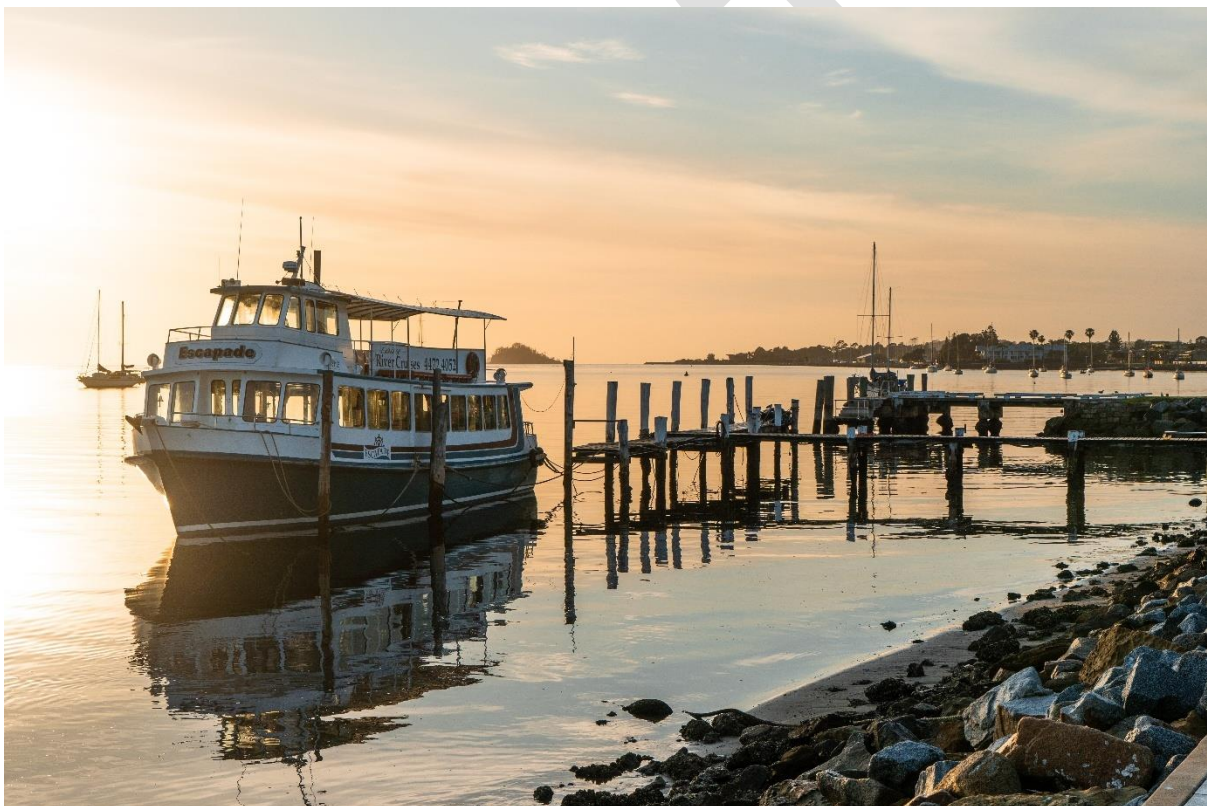


Figure 19 Jetty with boat docking

Credit: Destination NSW, Eurobodalla Coast Tourism

Currently, there is insufficient residential accommodation in the town centre. Increased residential densities in the town centre will create the foot traffic required to sustain a nighttime economy.

Tourism significantly impacts accommodation in Batemans Bay, often leading to the conversion of long-term housing into short-term rentals. Addressing housing and economic growth could enable the development of up to 1,800 new residential units across four sites.

The rise of online shopping complicates the demand for retail floor space. Retailers must meet consumer demands by providing both physical and online shopping experiences. It is estimated that 23,000m² to 27,000m² of additional retail space may be needed in the Tourism and Commercial quarters.

The town suffers from traffic congestion, emphasising the need to reduce heavy vehicle movements from the tourism quarter. A relocation of buses and trucks to the south of Beach Road would eliminate this congestion and support the intent of establishing the tourism quarter as a pedestrian friendly location.

The gateway sites on the southern end of the Clyde Bridge are crucial in establishing the town's visual identity, aspiring to beautifully designed spaces.



Figure 20 Tourism Gateway and Landmark sites

Challenges

The tourism quarter faces several challenges:

- a. Conflicting functions. The tourism and suburban retail functions are working against each other, limiting population and economic growth.
- b. Identity and Focus. The area's diverse purposes make it hard to establish a clear identity and strong brand. Balancing tourism, residential, commercial, and cultural needs is challenging. Finding common themes or values, like natural beauty and community spirit, could help create a cohesive identity.
- c. Traffic congestion. Traffic congestion can be alleviated through the relocation of bus bays and supermarkets.
- d. Parking. The need for above-ground parking due to flooding risks results in a need for taller buildings.
- e. Connectivity. Connections between key sites must be improved for pedestrian and cycle accessibility and flow.
- f. Retail evolution. Balancing retail floorplate requirements with the evolving changes from online shopping, which affects the viability of traditional retail spaces.
- g. Existing Development. Existing private development along the waterfront obstructs public access and use of the waterfront.
- h. Street Orientation. Orient Street is the main north south link between Clyde Street and Beach Road.

Positive outlook

There are opportunities to diversify the area by introducing cultural and recreational activities. The plan aims to integrate tourism and residential accommodations, commercial spaces, and cultural zones, along with ample off-street public parking to improve accessibility. These changes will enhance the vibrancy of the town centre.

Key sites

1 Smoke Point



Figure 21 Smoke Point

The area is a popular destination for both active and passive tourism, featuring short-term rental accommodation, recreational spaces, a skateboard area, public art installations, parking, a boat ramp, and houseboat moorings are located near the former jetty. Access to the nearby Murra Mia Walkway is difficult and constrained by existing developments.

Smoke Point has changed significantly since the bridge was built. Smoke Point offers a unique experience with its park, easy access to oyster farms, and the Clyde River. To the east, there are opportunities to diversify recreational use along the foreshore and Perry Street, including tourism, residential accommodation, commercial spaces, and cultural areas. This area also has the best off-street public parking, although traffic calming on Orient Street limits movement and connectivity.

Combining moorings with a jetty could create a temporary floating village featuring floating restaurants and cafés. These floating structures could be connected to land via jetties. Key considerations include managing environmental impacts and ensuring water quality. These would be temporary and rely on existing facilities.



Figure 22 Example of waterside dining with docks
Credit: Eurobodalla Coast Tourism

Relocating the NSW Department of Primary Industries facility next to the NSW Marine Rescue facilities at Hanging Rock. This is essential to enhance this location's potential for waterfront activation.

A potential development at the former Bowling Club site will require improved pedestrian and vehicle connectivity to reduce congestion at the intersections of Vesper Street, North Street and Beach Road.

To improve connectivity to Smoke Point from the Mackay Park locality to the oyster farms, the eastern retail and foreshore promenade, a shared pathway is proposed on the western side of Vesper Street, to redirect pedestrian and cycle traffic to the bridge underpass at Clyde Street, rather than crossing the Highway. This will improve connectivity whilst providing educational opportunities of the significance of the habitat along McLeod's Creek.

Clyde, Orient and North Streets will be transformed to become destinations rather than service localities. The two shopping centres have relocated the activity from the existing street frontages to within the centres. This plan aims to reinvigorate the streets and make them an active and thriving destination.

Opportunities include:

- A day and night economy on the waterfront.
- A fish market related to a jetty or a floating market by the houseboat mooring field.
- Utilising the Marine Fisheries site for a dining and function centre.
- A temporary café, restaurant, or bar on the bridge pylon during summer.
- A parklet around the stormwater detention area, providing a landing for the pedestrian pathway back to the former Bowling Club site.
- Redevelopment of the retirement village for high-density living.
- Redevelopment of the NSW Fisheries site as a food and entertainment destination.



Figure 23 Examples of adaptable reuse of vessels as a floating restaurant
Credit: Destination NSW, Eurobodalla Coast Tourism

2 Clyde Street



Figure 24 Clyde Street

This site is the eastern gateway into the town. Clyde Street, under the bridge, offers opportunities for increased height and beautiful places, connecting residents and tourists to Smoke Point, Murra Murra Mia Walkway, and retail areas along Perry and Orient Streets.

The bus interchange at Clyde and Orient Streets can be relocated to improve vehicle movement in the town. Options include, the commercial quarter, south of Beach Road or in the West Vesper Street locality.

The removal of heavy vehicles from the tourism quarter will allow for the beautification of streets and allow for greening.

The area under the bridge and the adjacent car and boat parking is ideal for a multi-purpose events space.

Opportunities include:

- Developing a nighttime economy along the waterfront with temporary leases for restaurants and bars from the park and under the bridge.
- Using the bridge substructure for a floating café or restaurant.
- Partially closing a lane to create a street and fish marketplace.
- Promoting the foreshore as an event space.
- Reducing the heavy vehicles reliance and demand on visiting the area.

3 West Vesper Street



Figure 25 West Vesper Street

Vesper Street/Princes Highway is the main thoroughfare transporting vehicles through Batemans Bay.

This precinct includes the western side of Vesper Street, the Bay Pavilions site, and the former Bowling Club site adjacent to Mackay Park.

The vacant land offers a chance to create a landmark building with views of McLeod's Creek, providing a unique experience different from the eastern side of Vesper Street. This land can accommodate a range of uses, including commercial, retail, tourism and residential opportunities.

Improving connectivity to Smoke Point through the use of a shared pathway, McLeod's Path, reducing pedestrian congestion on Vesper Street at North Street and Beach Road.



Figure 26 – Mangrove Boardwalk
Credit: Eurobodalla Coast Tourism

This location also provides an alternative site for the bus interchange. Relocating the bus interchange will improve traffic flow and create a more efficient arrangement for bus movements in the Clyde Street location.

Mackay Park allows for large events, such as sporting games and concerts, with the opportunity for nighttime events without disturbing nearby residents.

Opportunities include:

- Tourist and visitor accommodation with mixed uses, including serviced apartments, residential accommodation, restaurants, retail and commercial uses.
- Uses and facilities to complement the Bay Pavilions and boost the nighttime economy.
- Develop an economic strategy to activate off-season tourism using the sports field.
- New commercial spaces, including a potential Metro supermarket and facilities to complement the Bay Pavilions.
- Conferencing and education facilities.

4 Perry Street



Figure 27 Perry Street

Perry Street presents an opportunity to reduce building height from the taller structures along Vesper Street while incorporating blue-green infrastructure for climate-resilient pedestrian spaces.

The main goal for Perry Street is to transform the area into a town square, increasing pedestrian connectivity and removing the reliance on vehicles.

Key changes include prioritising shared pathways over vehicular access. Removing heavy vehicles and limiting access to medium-sized delivery vehicles only. Installing traffic calming measures.

By reducing the road width, this will create opportunities for public parks and landscaping.

Opportunities include:

- Conversion of the current car park in Perry Street to a landscaped park area providing linkages through to Orient Street and Murra Murra Mia Walkway.
- Provide outdoor dining opportunities within landscaped recreation spaces.
- Create an enclosed event space away from the foreshore and the natural coastal environment.
- Development of a public art and sculpture park.
- Provide areas for a public common allowing seating, lighting gardens which offer a different experience to the other parks and recreation spaces within the Town Centre
- Providing for residential accommodation as shop top housing.

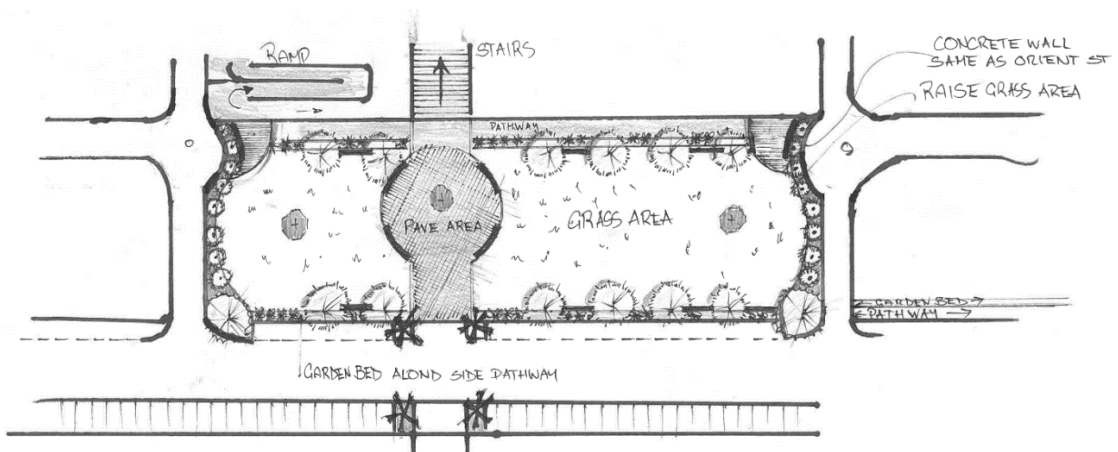


Figure 28 Perry Street urban green space concept

5 North Orient Street



Figure 29 North Orient Street

This area is situated between the Murra Murra Mia Walkway and the Perry Street carpark and extend from North Street to Beach Road.

This area is the traditional town centre. It consists of low-density commercial opportunities.

The precinct contains restaurants facing the Murra Murra Mia Walkway, however, these do not front or activate Orient Street.

Orient Street is a one-way street which causes traffic congestion. Heavy vehicles and cars dominate this street and the street is not a destination. Orient Street north will offer an “eat street” concept focussing on food and beverage establishments, with streetscape improvements enhancing connectivity to the commercial quarter. Opportunities for night markets and temporary outdoor dining spaces are available.

Opportunities include:

- Facilitate two-way traffic in Orient Street.
- Landscape the street to create a tree canopy.
- Reconfigure the entrance to the Mariners’s Hotel and add a walkway to the waterfront with ground level retail spaces.
- Provide paid off-street car parking at the southern end.
- Create legible east-west links to water and the village centre.

6 Murra Murra Mia Walkway



Figure 30 Murra Murra Mia Walkway



Figure 31 Murra Murra Mia Walkway form, shadow and massing



Figure 32 Historic Photo of Natural Swimming Pool at Batemans Bay

The historic working waterfront, public recreation areas, and traditional retail streets are vital destinations for locals and visitors alike. The Batemans Bay Waterfront Master Plan and Activation Strategy (Waterfront Plan) includes recommendations to enhance the town centre and activate this precinct.

A significant redevelopment opportunity exists at the existing Mariners Hotel site. The Waterfront Plan recommends a pedestrian linkage from the Murra Murra Mia Walkway to Beach Road at the existing traffic lights with Flora Crescent. This also offers a new vehicular link to Beach Road. This will allow for improved activation of Orient Street. This site also presents changes for multistorey parking facilities.

Historically, the foreshore contained an ocean swimming facility. A contemporary swimming facility will assist in activating this precinct.

This area is subject to sea level rise. The current RL is below that of a King Tide. Council studies have identified works to be undertaken to protect the CBD from inundation. Further work is being undertaken to define exactly how this construction will occur; however, it is envisaged that this work will allow for an increase in the width of the walkway and make it a multipurpose facility.

Opportunities include:

- rooftop recreation spaces on existing buildings, linking directly to the Walkway.
- a cantilevered raised outdoor dining roofed deck.
- Implementing a landscaping and lighting strategy.
- Consider the possibility of temporary moorings attached to the deck for boat and harbour transport.
- Create opportunities for events spaces.

2 Commercial Quarter

Overview

The commercial core of the town is centred on Orient Street / Old Princes Highway. The traditional town CBD is vital to the town's identity, with Orient Street south of Beach Road which now contains the main commercial hub.

The goal is to enhance this focus by establishing dedicated spaces for commercial premises and shop top housing.

To reimagine the town's CBD, lot consolidation will be necessary. Opportunities will then arise for the large expanse of at grade car parking areas.

There are several vacant lots, which are primed for future development opportunities, located along Orient Street.

The areas on the western side of the quarter and along the Old Princes Highway may transition from residential uses to productivity support land uses.



Figure 33 Commercial Quarter

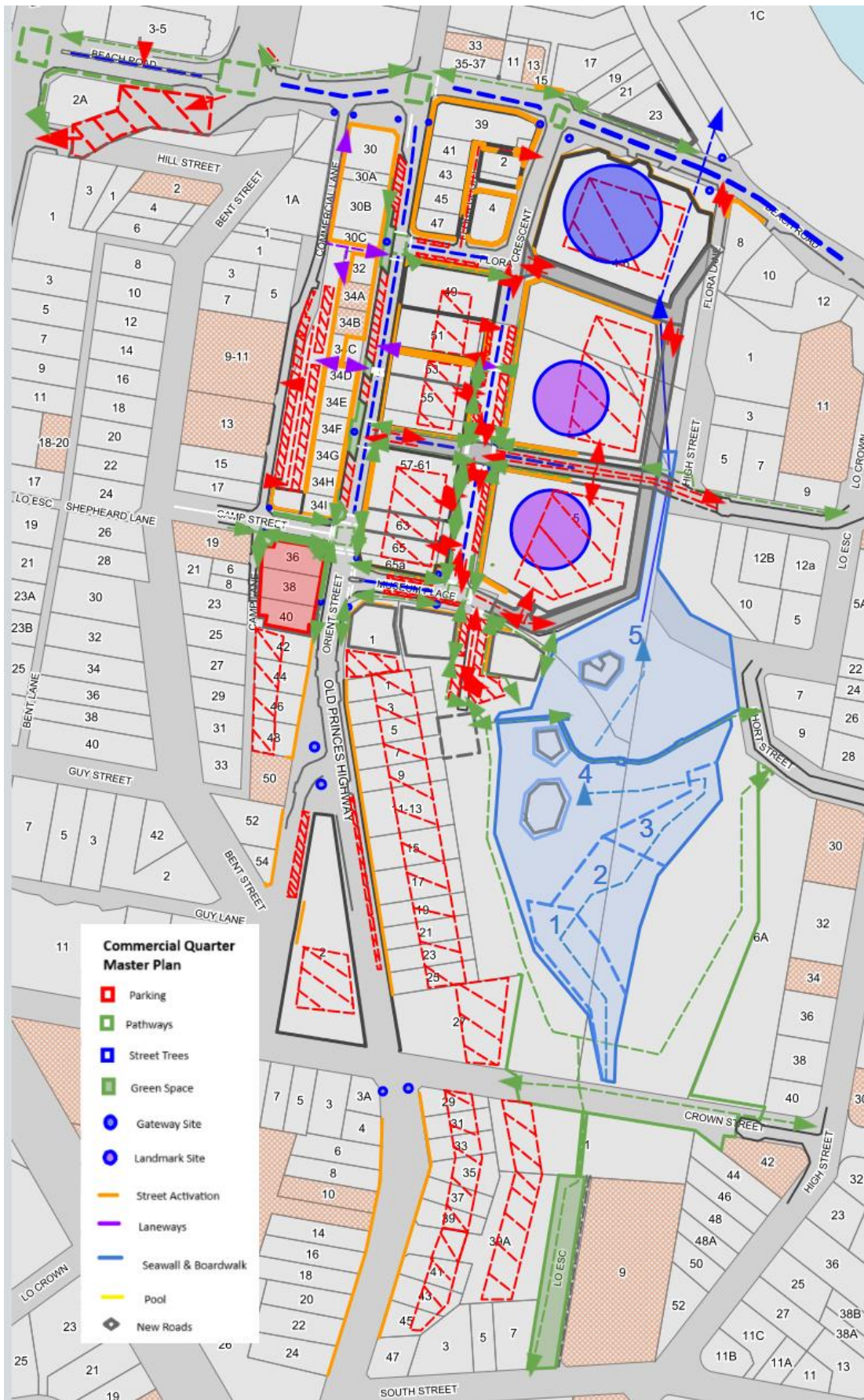


Figure 34 Master Plan – Commercial Quarter

Limited small lots on the western side restrict development, where motor vehicle businesses prevail. Professional services are concentrated along the old Princes Highway and Orient Street, potentially transitioning from R3 Medium Density Residential to MU1 Mixed-Use zone.

Challenges

The commercial quarter challenges include:

- a. Historical Shift: The town's core shifted from South of Beach Road to areas near the old bridge, leading to a decentralised commercial focus and creating a second centre.
- b. Compartmentalisation: The CBD experiences compartmentalisation of services, with distinct commercial, retail, and industrial quarters, making integration challenging.
- c. Large-Scale Development: Recent large-scale developments have centralised spending activity. The town centre remains tranquil after business hours, lacking a 24-hour business cycle.
- d. Suburban Connectivity: Improving suburban shopping and food retail connectivity with residential areas is essential to reduce traffic congestion around North, Perry, and Orient Streets and Clyde and Beach Roads.
- e. Supermarket Relocation: The current supermarket locations detract from street side shopping activity and draw people into the centre. Relocating shopping centre activity will reduce traffic congestion and parking issues. This is a critical structural change for the town's sustainability and economic growth.
- f. Aging Infrastructure: Many buildings from the 1970s and 80s need revitalisation or replacement, showing a trend for regeneration and redevelopment.
- g. Economic Pillars: There is a lack of clarity about the commercial centre, with isolated major government departments and office services. Attracting professional services from Sydney and Canberra is challenging due to a lack of quality office space.
- h. Tourism and Retail Integration: Reinvigorating the town's economic sustainability requires creating an authentic, context-based reason for tourists to stop and linger, beyond just food and hospitality.
- i. Delineation and Density: The plan aims to integrate commercial and tourism areas with residential areas and transition the town centre from low-density to higher-density development.
- j. Living Town Centre: The absence of people living in the town centre leads to commercial decay. Encouraging mixed-use development and higher-density living is crucial.
- k. Nighttime Economy: Establishing a vibrant nighttime economy that is sensitive to the coastal setting and supports the growth of the nighttime recreational and entertainment area is essential.
- l. Environmental Measures: Revisiting noise attenuation and environmental measures is necessary to support the commercial quarter's growth.
- m. Parking Solutions: Rationalising vehicle parking through vertical consolidation in structured parking facilities is preferred over horizontal sprawl.
- n. Multiple Ownership: Consolidation and redevelopment are constrained by multiple ownerships and the absence of a local champion to guide a vision for the town centre.

Positive outlook

The gateway locations at the Batemans Bay Soldiers Club and Mariners Hotel offer opportunities for height and architectural symmetry. As the CBD develops, car parking in this area and the tourism quarter needs evaluation to enable adequate future supply. The Eurobodalla Local Infrastructure Contributions Plan 2022 applies to part of the commercial quarter.

Multi-level public parking and mixed-use commercial developments are crucial for economic investment. The Flora Crescent and Beach Road intersection needs reassessment due to expected traffic from the tourism quarter.

Orient Street features single-storey retail, older two-storey buildings, undeveloped lots, and large parking areas that detract from its visual appeal. Key sites will become economic generators as density increases. Introducing mixed-use typology will activate the area and improve the economy. Multi-level public parking may be needed to the east of the quarter.

Orient Street south will offer an "eat street" concept focusing on food and beverage establishments, with streetscape improvements enhancing connectivity to the tourism quarter. Opportunities for night markets and temporary outdoor dining spaces are available next to Commercial Lane.

The Water Garden is an essential location for connecting Museum Place and Crown Street to Beach Road and the existing shared pathway sculpture walk, integrating art with the coastal setting. The former Court House Museum area offers possibilities for recreational spaces that merge heritage and commercial uses with recreational spaces.

East of Orient Street, an open-air car park represents an untapped opportunity for future commercial land. The council-owned car park at Orient Street and Camp Street is a strategic asset, with potential for a multi-storey building to accommodate parking demand.

Key sites

1 Highway layover and Bus stop



Figure 35 Highway layover and Bus Stop

The eastern side of Vesper Street and Princes Highway has businesses like McDonald's, a car wash, a building company, and a community services provider. McDonald's is a key stop for travellers due to limited facilities open after 5 PM. Residential dwellings conflict with the area's objectives, and there's no cohesive vision for its future.

Access from the Beach Road roundabout creates shortcut traffic and is poorly lit, contains isolated parking with EV charging facilities. This area could be reimagined as a bus interchange or truck stop with additional fast-food options, a dedicated slip lane, and improved visibility.

Relocating bus stops from Clyde Street to this area is recommended, with southbound buses accommodated with minimal changes and northbound buses in the Mackay Park precinct. Enhancements like better lighting, signage, and Indigenous art could elevate the area.

Opportunities include:

- New interstate bus interchange.
- Intensive quick stay food and supply outlets.
- Improved road ingress and egress safety.
- Street landscaping.
- Lot consolidation for improved building connectivity.

2 South Orient Street



Figure 36 South Orient Street

Large vacant areas in town offer redevelopment opportunities to revitalise the CBD along Orient Street. These sites could include supermarkets, offices, and shop-top housing. Effective vehicle parking solutions are needed, with most off-street, either in podiums above the second storey or at the rear of key sites. A dedicated parking structure is still necessary.

Parking structures, often seen as unattractive, could be improved by wrapping the ground floor with retail or office spaces. The Council's current public parking at Camp and Orient Streets has around 130 spaces. With population growth and CBD revitalisation, this site could add levels—potentially five stories—to meet 10% of future parking needs, easing pressure on the tourism quarter.



Figure 37 Examples of angle parking suitable for Orient Street south, Flora Crescent

A street tree planting program can enhance Orient Street's appeal and commercial value. The 20-metre width allows for both parallel and angled parking, though parallel planting

might reduce spaces. Clear pedestrian crossings with low hedges will support safety in the 40 km/h zone from Camp to Clyde Street.

The western side of Orient Street can have angled parking, while the eastern side may have limited parallel parking due to on-site parking at businesses like Dan Murphy's. The underutilised Dan Murphy's car park could be redeveloped into an internal parking structure or connected to the Soldiers Club superblock car park.

Opportunities include:

- Construction of large scale multi storey car parking facilities.
- New stormwater management systems.
- Improved traffic management and car parking.
- Localised height provisions to incentivise shop top housing.
- Laneways and street art.

3 Soldiers Club



Figure 38 Soldiers Club

The 2.3-hectare Soldiers Club southern car park site, next to the office block and club facilities, has a hillside backdrop allowing for taller buildings. Multiple frontages promote pedestrian access and connectivity.

The southern and western frontages should vary in height with the land's contours, and the car park offers height opportunities. The superblock can be subdivided for large-scale CBD office and commercial use, ensuring no net loss of parking with rear-loaded multistorey parking set back from Flora Crescent.

The site can handle increased vehicle movements with access from Flora Lane, Hill Street, Beach Road, Flora Crescent, and Museum Place. It could host a central office park with a grid pattern promoting pedestrian traffic and connecting via Dan Murphy's parking areas.

Redirecting the drainage course from the Water Gardens to the rear would extend Flora Crescent to Museum Place, enhancing retail opportunities and public access to the Gardens.

The Soldiers Club sites have significant commercial development potential. Increasing the building scale along Orient Street could attract more shoppers. Enhancing pedestrian connectivity will require redesigning key intersections like Beach Road at Flora Crescent and Orient Street at Museum Place for better traffic flow.

Opportunities include:

- Create a third east/west transport link.
- Redevelop the Soldiers Club to include a mixed use development including shop top housing.
- Redirect and redevelopment the watercourse and stormwater system from the Water Gardens.
- Reconfigure Flora Lane as a water course landscaping and pedestrian thoroughfare.
- Construct a large scale multi use building on the car park.

4 Camp Street



Figure 39 Camp Street

This Council-owned site offers accessible car parking and is key for a structured parking facility to meet future CBD and Orient Street South needs. The plan includes a 5 to 6 storey facility to support town centre growth.

While not economically viable alone, parking structures provide value by supporting commercial redevelopment and reducing congestion. Costs should be covered by a development levy on all commercial redevelopment, especially where on-site parking is insufficient.

5 The Water Gardens



Figure 40 The Water Gardens

The Water Gardens is the central basin of the town's water catchment, functioning as a vital water retention space amidst an urban landscape.

The Water Gardens is a known Grey Headed Flying Fox camp, meeting the criteria for a Nationally Important camp. The Water Gardens is dominated by casuarina which provides core roosting habitat for the GHFF. Camp vegetation is mapped Swamp Oak Floodplain Forest Endangered Ecological Community.

This area is the town's most significant parcel of blue and green infrastructure. An informal pathway connects South Street to Crown Street and Museum Place along the back of properties facing Old Princes Highway. The Museum, located in the historic Court House buildings, is nearby.

A shared pathway links the Water Gardens to High and Crown Streets. The Water Gardens enhance the townscape and quality of life for residents and visitors, playing a key role in managing water through the catchment.

Although Crown Street does not currently extend to the Water Gardens, its future extension will be important for east-west traffic. This extension will improve access to large residential sites and relieve transport pressures. Once completed, the south side of Crown Street may be rezoned from C2 to RE1 Public Recreation.

Opportunities include:

- Creating a town park.
- Relocating the Museum to a purpose-built facility as part of a shop-top development on the courthouse site.
- Enhancing recreational space and accessible green space at the Water Gardens.
- Constructing the Crown Street unformed road reserve to connect the Princes Highway to Pacific Street.
- Introduce Crime Prevention Through Environmental Design principles.
- Create a purposeful destination such as a music bowl on the north-western side of the Water Gardens.
- Improve water quality.

6 Old Princes Highway



Figure 41 Old Princes Highway

This area is currently zoned for residential use and contains an additional permitted use enabling the establishment of professional services. The additional permitted use allows for funeral homes, information and education facilities, office premises, public administration buildings, and veterinary hospitals. The move from residential to MU1 mixed-use zone will continue to develop. A realignment to support this transition is considered appropriate.

The changes following the additional permitted use granted in ELEP 2012 have strengthened the Central Business District as a professional office zone. Integrating various retail activities from the residential area into the mixed-use retail hierarchy on Orient Street would require MU1 rezoning. There is no need to alter the current height limits for this purpose.



Figure 42 Old Princes Highway transition from residential to mixed-use

Opportunities include:

- Change the zone to facilitate a move toward employment opportunities.
- Facilitate shop top housing.
- Develop a landscaping / streetscape plan for Old Princes Highway to introduce a tree canopy and boulevard feel.

3 Residential Quarter

Overview

The residential quarter is the largest area, stretching 4 km along the foreshore from Albert Ryan Park to Batehaven. It includes ridges and valleys accessible from Pacific Street, Bavarde Avenue, and Country Club Drive, with sea and Clyde River views from Bent Street and High Street.

Access is limited by Beach Road from the north, with informal routes from the south. The area centres around Catalina Golf Club and Marina, extending to Batehaven commercial centre and Corrigan's Beach, offering housing, community services, a school, and sports fields. There are prospects for higher-density housing at key sites.

Zoned R3 Medium Density, there are nine growth opportunities, but the area's character is limited by seven-storey developments and intensive redevelopment. R3 zoning often leads to strata titling, restricting future redevelopment.

The Batemans Bay Marina development includes a low-density resort, Marine Rescue facility, and Yacht Club, promoting sports and recreational boating.



Figure 43 Residential Quarter

Challenges

The residential quarter challenges are:

- a. Identity and Focus: The areas diverse purposes make it hard to establish a clear identity and strong brand.
- b. Traffic Congestion: Significant traffic congestion issues, which could be alleviated by introducing a congestion zone and relocating supermarkets.
- c. Parking: The need for above-ground parking due to flooding risks, which complicates development and increases the need for taller buildings.
- d. Connectivity: Ensuring effective connections between key sites, such as pedestrian and cycle pathways, to improve accessibility and flow.
- e. High water table and flooding issues: Ensuring coastal protection and inundation management protect residents and businesses from flood and stormwater inundation.

Positive outlook

The elevated residential ridges offer stunning views and a vibrant community atmosphere with the marina, golf course, and sports fields. Safe recreational areas enhance public spaces and quality of life. Transport networks are being improved for better accessibility, and local roads are pedestrian-friendly. The marina resort and Catalina Club car park sites allow for diverse building heights that blend with the landscape.

Higher-density residential options will be near the town centre, maintaining suburban character by limiting growth to central sites. Infill housing opportunities exist around Golf Links Drive and Bavarde Avenue. There is a need for one- and two-bedroom apartments, encouraged by building height incentives. Overall, there's potential for a mix of commercial, residential, and health-related developments.

Key sites

1 Bent Street



Figure 44 Bent Street

Bent Street's elevated location near the town centre offers easy access to commercial areas, health facilities, and the foreshore, with great views. Several older sites need renovation,

presenting opportunities for demolition and lot amalgamation. The area, on a 20-meter natural podium with mature trees, allows for landmark buildings and increased housing density. There are prospects for a mix of commercial, housing, and health-related facilities, with potential car parking at the base of the hill.

Opportunities include:

- Increase the height provisions to facilitate 40m height above natural ground level.
- Improve pedestrian connectivity to Beach Road and the tourism quarter for foot traffic and mobility devices.
- Introduce a long term landscape plan for the street.
- Create a hill top viewing location.

2 Beach Road



Figure 45 Possible built form along Beach Road

The Beach Road precinct extends for 4kms from the Soldiers Club to the signalised intersection at Batehaven. A reconfigured Beach Road and foreshore offers the chance to create a landscaped boulevard that enhances walking, cycling, and passive recreation, connecting the tourism quarter to the promenade to the Marina redevelopment.

The foreshore is essential as a main east-west corridor and extends the Murra Murra Mia Walkway. While density along Beach Road and Golf Links Drive is already accommodated by current height controls permitting seven-storey buildings, a new height limit of 0 to 20 metres (RL 1) would offer minimal additional density. In contrast, a limit of 20 to 40 metres

(RL 2) could lead to high-rise developments that create a "wall" effect. The proposed 'wave form' approach would allow varied heights, ensuring buildings are set back from the foreshore and in harmony with the landscape. Additional height may be feasible near the golf course to provide sea views over existing homes.

This initiative aims to enhance water access and improve Beach Road's driving experience through a cohesive streetscape with consistent tree plantings. Although rising traffic is anticipated, Beach Road also serves as a key sea wall defence, which limits direct tree planting. However, increased road height could allow for centreline street trees. Consistency in tree placement and species is crucial for creating an iconic atmosphere.

Opportunities include:

- Develop the road as a boulevard destination, activating pedestrian and bike paths.
- Landscape can create a coastal interface.
- Create a diversity of heights.

3 Bavarde Avenue



Figure 46 Bavarde Avenue

This medium-capacity residential site, located at the base of a hill, includes Herarde Street and Heradale Parade. The contours present an opportunity for increased height development, with taller structures aligned with the steep hospital site above. Consolidating blocks would enable increased height and density for tourist or serviced apartments south of Beach Street. This area offers excellent northerly views of Batemans Bay, views over a golf course to the south, and easy access to the town and marina.

Opportunities include:

- Increased height with significant increases to 40m adjacent to the hospital.
- Increase existing heights to 20m between Heradale and Herarde Streets.
- Create a diversity of heights that compliments the hills.

4 Batemans Bay Marina



Figure 47 Batemans Bay Marina

This site should be viewed as part of the marina resort (key site 5). The marina is currently at full capacity with around 190 berths, but its capacity is expected to double under the master plan. Its location between Sydney and Eden serves as a crucial safe harbor for coastal sailing and supports local boating, yacht racing, and maritime services.

To accommodate future users, additional berthing and jetty space will require excavation at the southern end of the marina. The creek dividing the marina receives stormwater runoff from the residential area and golf course, contributing a low level of sediment that will need careful management. Maintaining a low height along the residential boundary is important for ensuring privacy for vessels and their users.

Opportunities include:

- Expansion of the boat berthing facilities.
- Improvement to pathways linking public spaces.

5 Batemans Bay Marina Resort



Figure 48 Batemans Bay Marina Resort

This 6-hectare site, zoned SP3 Tourist and R3 Medium Density Residential, presents an opportunity for a diverse quayside housing development that connects boating and residential activities to harbor views. It allows for buildings up to five stories (RL1), with higher structures at RL2 and RL3, located central to the site.

The existing marina facilities to the north suggest potential expansion for increased berth capacity. An expanded marina would enhance residential and tourism opportunities linked to a quayside development.

Revitalising the locality will create public and private spaces next to jetties and boardwalks, representing a significant investment in tourism. Augmenting the southern jetty with private jetties can relieve pressure on northern marina facilities.

The plan also includes improving the foreshore promenade to better connect to the seawall and rock fishing areas. This quayside space offers great potential for community events, integrating apartments, jetties, and boating facilities alongside Hanging Rock.

Opportunities include:

- Improvements to the foreshore promenade to better connect the seawall and rock fishing areas.
- A multi height quayside style development containing mixed use buildings.
- Construction of a promenade linking Beach Road around the waterside to the Rock Wall.
- Promote mixed use development along the waterside frontage, including dining opportunities and facilities supporting the adjacent recreational fishing areas.



Figure 49 Potential height and buildings at the Marina Site

DRAFT

6 Catalina Golf Club car park and Golf Links Drive



Figure 50 Catalina Golf Club and Golf Links Drive

The car park site, measuring 8,792 square metres at the end of Golf Links Drive, has access to Beach Road. There are opportunities to enhance parking capacity within the 62-hectare golf course and repurpose the existing car park for apartment and hotel accommodation. This plan supports increased building heights, aligning with the wave approach, and allows for a landmark site that complements the marina resort's heights. Although close in proximity, sufficient separation lets them remain independent yet part of a cohesive design. Golf Links Drive is experiencing residential redevelopment with buildings up to seven stories, raising concerns about potential view loss for adjacent residents unless new developments incorporate stepped heights.

Opportunities include:

- Increased building heights.
- Create a landmark location.
- Diversity in heights to protect views.

7 NSW Marine Rescue Facilities



Figure 51 NSW Marine Rescue Facilities

The site serves as an operational rescue facility and features a public jetty, boat ramp, wash-down facilities, and trailer boat parking, centred around the two-storey Maritime Rescue Building. This existing Department of Primary Industries (DPI) facility operates under the Eurobodalla Local Environmental Plan (LEP) 2012, which imposes no height limits due to its designation for recreational and public operational use and protect navigational markers.

Spanning 3.6 hectares, the site has a large parking area and designated wash-down facilities within recreationally zoned land that prevents commercialisation.

Access is provided via Hanging Rock Place and Tuna Street, with potential to redesign the foreshore area and develop a shared pathway connecting to Beach Road, enhancing access to nearby rock fishing sites.

The nearby yacht club, which includes a clubhouse and secure boat storage, is isolated from other boating facilities and inhibits observation of ocean racing.

Opportunities include:

- Creating a boating hub, collocating Marine Rescue, DPI and the Yacht Club.
- Improving visual access to ocean racing events.
- Relocate the DPI Fisheries Protection Marine Unit from Smoke Point.

8 Hanging Rock Sports Fields



Figure 52 Hanging Rock Sports Fields

The recreational role of Hanging Rock as a sports hub complicates redevelopment beyond upgrading the sports field. The site is suitable for a stadium and additional facilities, bordered by a remote-controlled car racing area, a Council material stockpile, and the yacht club. Relocating the yacht club and stockpile could create space for ancillary sporting facilities to the south of the oval. This project primarily aims at beautification with minimal changes to existing activities, consolidating functions, and improving building capacity.

Opportunities include:

- Create a space for ancillary sporting facilities south of the oval.
- Consolidate functions.
- Improve building capacity.

9 Batehaven

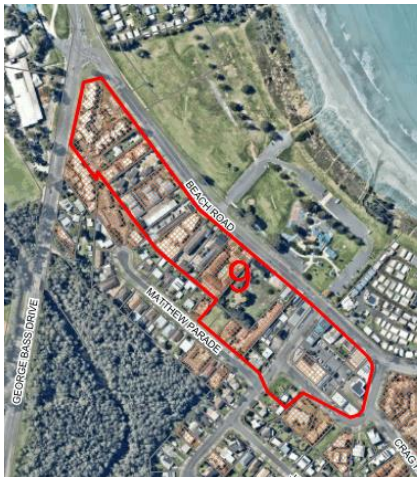


Figure 53 Batehaven

This redevelopment area complements the local commercial centre by enabling moderate height increases to promote economic sustainability and retail revitalisation. Located near mountain bike trails, the site at Curtis Road in Batehaven has growth potential for the centre and surrounding residential areas.

The Corrigan's Beach site presents a beachfront opportunity to transform the centre into a community hub for events like Mogo Trails. Positioned between suburban and urban growth areas, it can cater to immediate shopping needs, with traffic and parking designed for peak summer times. Furthermore, it will serve as a launch point for Mogo Forest Mountain Bike trails, offering accommodations, retail, and bike servicing to enhance economic growth.

Opportunities include:

- Create a centre for a community hub.
- Increased height to diversify the built form.
- Connect the Mogo Bike Trails to a local service centre.

4 Industrial Quarter

Overview



Figure 54 Industrial Quarter

The E4 General Industrial and E3 Productivity Support areas have a long history of industrial uses, such as concrete batching and automotive repair. Recently, retail activities have grown in the E3 zone, with fast food and truck stop cafes emerging, driven by visibility and accessibility from the highway.

This area is evolving into a mix of retail and industrial uses, particularly along Vesper Street from Beach Road to Guy Street. The eastern section around the Vesper Street intersection features a mix of older industrial trades and late-night dining options.

Characterised mainly by single-storey retail and automotive businesses, the industrial quarter faces challenges due to limited land supply and high demand. The future plan aims to shift focus from heavy industrial activities to a diverse mix of retail and industrial spaces, as the town centre densifies.

Large allotment sizes and high land values hinder traditional industrial use, but opportunities for subdivision into smaller spaces for light industrial technology may arise. The area will continue to support the commercial centre while promoting collaboration between industrial and retail sectors.

Key components of the future plan include:

- - An office park
- - A public transport hub
- - A local suburban commercial centre

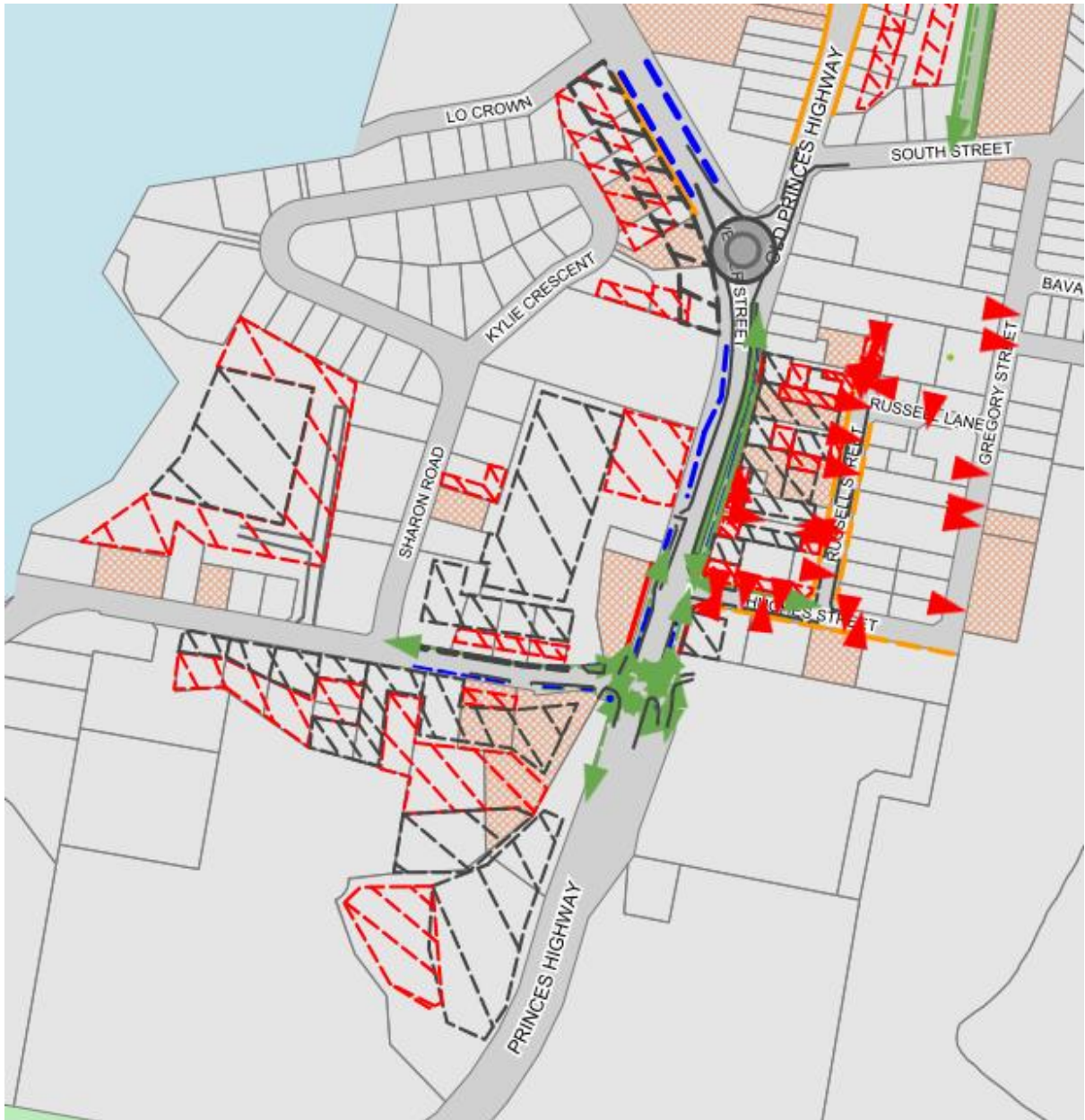


Figure 55 – Master Plan – Industrial Quarter

Challenges

Of all the quarters in Batemans Bay, the Industrial quarter is the most constrained and problematic. Industrial development is moving north toward the commercial quarter of town. Most developable blocks have been almost fully utilised. The Industrial quarter is highly constrained by the Mogo State Forest to the south. The western area is adjoining coastal wetlands to the north. The land situated between the Princes Highway and Cranbrook Road is undevelopable due to the natural topography of the land.

The Industrial quarter challenges are:

- a. Land Supply Constraints: The industrial quarter is limited by available land, making it difficult to expand land for industrial purposes.
- b. Transition to Retail: The shift from industrial to retail uses, especially in the E3 zone, is reducing the space available for traditional industrial landuses.
- c. Infrastructure Needs: The redesign of highway intersections and the need for better landscaping and greening incentives are necessary to improve accessibility and the visual quality.
- d. Environmental Regulations: Adapting to changing environmental regulations and ensuring sustainable development can be challenging.

Positive outlook

The emphasis on expanding retail presence along the Old Princes Highway in the Commercial Quarter, along with easy access to the highway, will enhance the vitality of the area and promote a dynamic business environment. The reinstatement of a CBD with commercial office spaces as part of the MU1 Mixed Use zone is expected to encourage smaller boutique professional services and retail conversions in the residential area situated between the southern E4 General Industrial zone, east of Vesper Street, and the expanded commercial area.

Meanwhile, the industrial quarter presents an opportunity to explore potential locations for a new industrial area, accommodating future growth beyond 2030. This local requirement aligns with the broader need for additional industrial land in Eurobodalla.

Key sites

1 Western Industrial

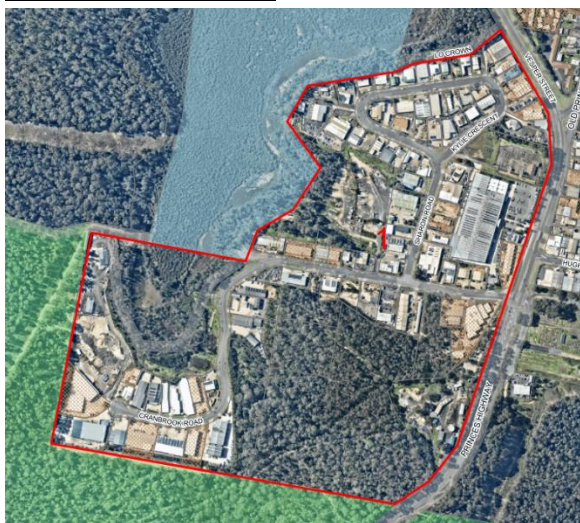


Figure 56 Western Industrial Area

This area is linked to the food retail sector near the MU1 zone and benefits from its location next to the southern entry point of the town via Old Princes Highway, serving both highway traffic and the Tourism Quarter. It acts as an alternative stop for late-night travellers and has high visibility, making it appealing for commercial use. The existing industrial backdrop is not ideal for a welcoming town entry.

The existing zoning is suitable, but enhanced landscaping is needed to create a more inviting gateway. Relocating residential uses from this site is essential, as the area trends towards employment, and more industrial development could harm nearby residential areas, undermining the desired suburban feel.

The confusing three-way intersection and slip road need rationalisation, potentially through redesigning it as a roundabout to improve service access. Limited site space is influenced by the surrounding topography, suggesting that increasing building height on deeper lots may be feasible, paired with landscaping incentives to balance the additional height.

Opportunities include:

- Prepare a landscaping plan to improve the visual experience along the Princes Highway and Vesper Street alignments.
- Consider development controls that inform and improve design along the Highway corridor.

2 Eastern Industrial Area



Figure 57 Eastern Industrial Area

The industrial area east of the Princes Highway has transitioned from residential to a mix of retail and semi-retail through adaptive reuse. It connects the town's CBD via the Old Princes Highway and Orient Street. The main focus in this area is on employment. However, this growth has impacted the industrial areas to the south and west. The area may continue to evolve toward a mixed-use designation, potentially shifting from an E4 zone to MU1.

The boundary between commercial and industrial uses is likely to move northward as the CBD redevelopment progresses. Ultimately, the area may shift toward retail and commercial uses, especially as development occurs near Camp Street and planned public parking. Strengthening the business-commercial identity of the existing industrial locality is essential to enhance its character and support the retail core without competing directly.

Opportunities include:

- A shift toward retail and commercial uses.
- Slip road rationalisation.
- A roundabout at the Old Princes Highway and Vesper Street / Princes Highway intersection.
- Prepare a landscaping plan to improve the visual experience along the Princes Highway and Vesper Street alignments.
- Consider development controls that inform and improve design along the Highway corridor.
- Potential for expansion of the cemetery to the south.

Actions

1 Tourism Quarter

The opportunities on land and water can be found in the MU1 Mixed Use or W2 Recreational Waterways zones. The extent of a new W2 zone at Smoke Point will need to be determined around the bridge, the existing fisheries protection site, the jetty and moorings. The following table of tourism opportunities for a mix of developer, community and council-led initiatives that aim to improve the quality of life and beautiful spaces within the town.

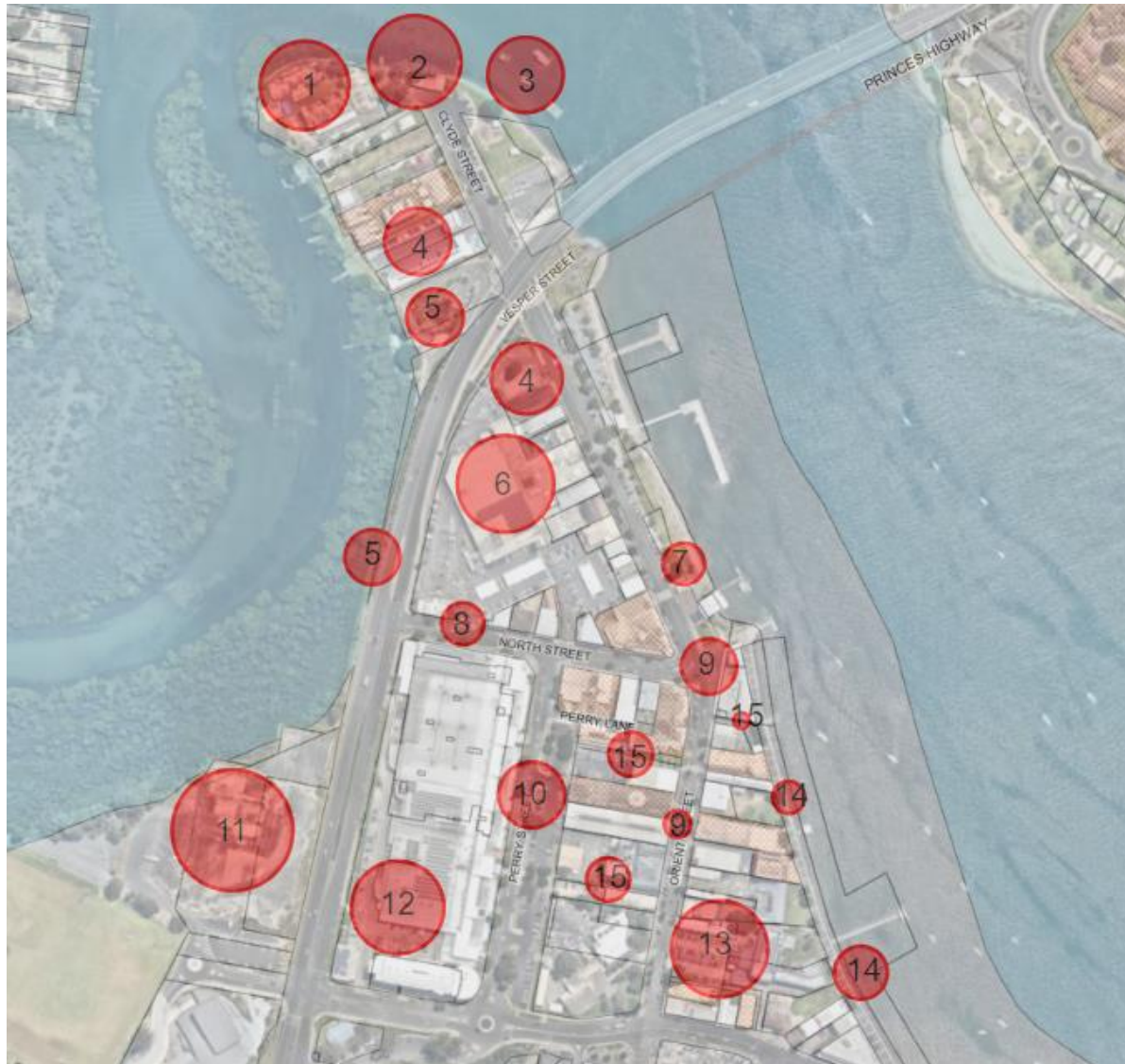


Figure 58 The Tourism opportunities

Site	Opportunity	Change	Theme
1.	Aged care, density, shop top housing	Increase height, rezoning R3 to MU1, increase height RL2.	Flourishing Morphology Typology Ecology Capacity
2.	NSW Marine Fisheries adaptive reuse Oyster fisheries retail, café, restaurant uses.	Relocate the use to hanging rock recreational boating hub and collocate with Marine rescue and the Batemans Bay Yacht club storage facilities.	Flourishing Morphology Typology Ecology Capacity
3.	Floating village, temporary commercial, jetty, mooring field infrastructure services	Rezone W1 to W2, temporary uses under bridge	Flourishing Morphology Typology Ecology Capacity
4.	Gateway building sites, Shop top housing, hotel, symmetry landmarks.	Rezone R3 to MU1, Increase height RL5, symmetry and sculptural goals and policy controls for each side of the bridge.	Flourishing Morphology Typology Capacity
5.	Mangrove shared path and bridge parklet	Remove fences, secure key infrastructure locations, landscape open space, rezone RE2.	Flourishing Morphology Ecology Capacity
6.	Shop top housing	Relocate Woolworths, Vespers Street HGV and parking off ramp. Increased height RL4. Vesper Street boundary landscaping upgrade.	Flourishing Morphology Typology Capacity
7.	Toilet block update.	Reorientate and upgrade facility and adjacent entry.	Flourishing Morphology Typology
8.	Control traffic movement, North Street partial closure, redirection of traffic and Village centre parking exit	Beach Road and Orient Street intersection lanes and traffic light reconfiguration, HGV decongestion zone, street tree planting, exit only option pedestrianised shared space	Flourishing Morphology Capacity
9.	Intersection and Orient Street upgrade, Street tree planting, exit only option pedestrianised shared space.	Relocate bus lay over site, street tree planting consistency, street tree fairy lighting, dual vehicle movements.	Flourishing Morphology Ecology Capacity
10.	Perry Street Park	Close the part of Perry Street between the car park entry roundabouts to traffic, create a green space parklet. Street tree, shared path pedestrianisation of service delivery access for Orient Street traders, HGV decongestion zone	Flourishing Morphology Typology Ecology Capacity
11.	Bowling Club mixed use hotel, shop top housing, metro supermarket and car park building	Rezone RE2 to MU1, Increase height to RL5. Shared path connection to smoke point.	Flourishing Morphology Typology Ecology Capacity
12.	Shop top housing, Village centre car park.	Increase height to mix of RL4, RL5 and RL2. Improve Vesper Street parking entry, added parking exit to North Street. Transition spaces: private and public realm	Flourishing Morphology Typology Ecology

Site	Opportunity	Change	Theme
		improvements. Align car parking entry with pedestrian movement connections to laneways.	Capacity
13.	Mariners landmark shop top housing, car parking building and conference hotel.	Increase height to RL4 or RL5, reconfigure Beach Road and Flora Crescent site access, pedestrianisation and stormwater upgrades. Symmetry and sculptural objectives and policy controls for each side of the Beach Road entry (Soldiers Club height (RL4 or 5)).	Flourishing Morphology Typology Capacity
14.	Boardwalk dining, ocean swimming pool, sea protection works	Increase the promenade width, add a 5m board walk cover of the sea wall, extend the dining opportunities out to the water. Locate a summer ocean swimming pool, connect jetty to pool facility.	Flourishing Morphology Typology Ecology Capacity
15.	Laneway safety, Temporary use spaces.	Improve views and access points, lighting, and landscaping of laneways.	Flourishing Morphology Capacity

Table 1 Tourism opportunity sites

DRAFT

2 Commercial Quarter

The following table of commercial opportunities and proposed changes offer a mix of developer, community and Council led initiatives that aim to improve the quality of life by providing beautiful places in the town.

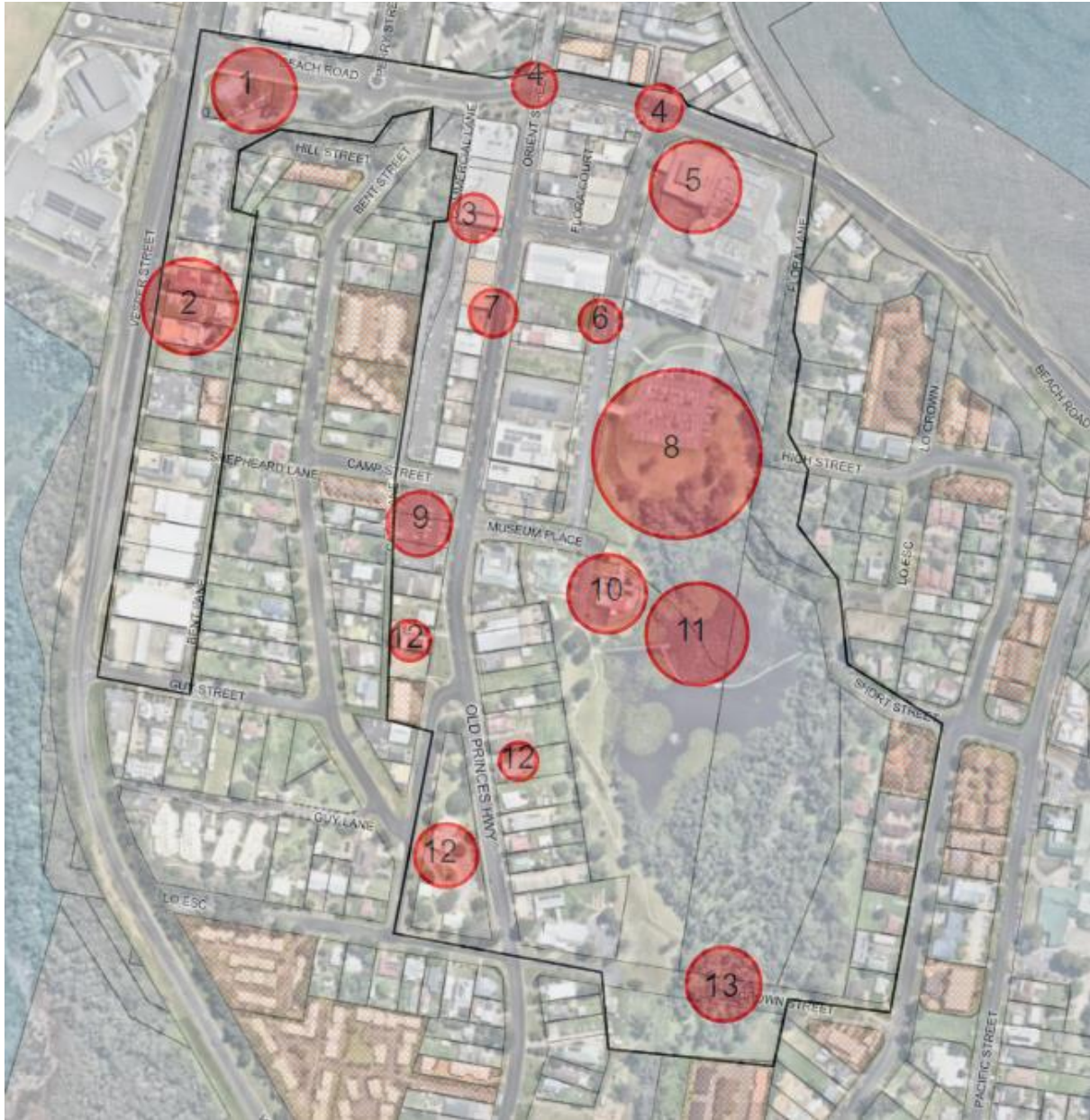


Figure 59 The Commercial opportunities

Site	Opportunity	Change	Theme
1.	Bus lay over Location	Relocated Orient and North Streets, Clyde Road bus stop layover with directional signage and locality street art.	Flourishing Morphology Capacity
2.	Highway truck stop layby	Creation of a late-night interstate tourism truck stop with services and facilities.	Flourishing Morphology Capacity
3.	Laneway upgrades to Commercial Lane and access laneway to Orient Street south.	A key connection route through to the rear car parking site, the car parking area offers future market space opportunity and a development site for retail office or shop top housing space.	Flourishing Morphology Capacity
4.	Intersection upgrades on the two Beach Road north south CBD connections	Orient street and Beach Road, Flora Crescent and Beach Road intersection upgrades.	Morphology Capacity
5.	Soldiers Club facilities site, hotel and retail enhancements.	Gateway site, increase height to RL4 or 5 symmetry and sculptural location approach (like Mariner's site in Tourism Quarter - see table 6). Symmetry and sculptural objectives and policy controls apply to each side of the Beach Road entry to CBD (matching Mariners' landmark height). Beach Road frontage with main activation toward Flora Crescent.	Flourishing Morphology Typology Ecology Capacity
6.	New road. Secondary main road CBD grid pattern, and street tree planting.	gazettal and naming of public road. Main Street frontage to the Soldiers Club facilities and Soldiers Club car park superblock site opportunities	Morphology Capacity
7.	Orient Street South	Duplication of Orient Street north with angle parking (west side), street trees and lighting, stormwater management upgrade and connection to potential new discharge via Flora Crescent intersection or Soldiers club network.	Morphology Capacity
8.	Soldiers Club car park superblock	Increase height to RL5, primary location for shop top, Metro supermarket and CBD office park. A central road corridor bisecting the block to align with the Dan Murphys car park entry. Requires pedestrianisation, drainage channel realignment and stormwater upgrades.	Flourishing Morphology Typology Ecology Capacity
9.	Public vehicle parking Camp Street	Increase height to RL3, location for the multi storey vehicle parking building. Access via Camp Street only.	Morphology Capacity
10.	Heritage Museum shop top and office CBD, relocation and adaptive reuse of the heritage listed building.	Increase height for office, apartments, and rehousing Heritage Museum (tenancy), relocation of heritage Court building to the Water Gardens. Zone changes from R3 to MU1, Increase Height to RL3.	Flourishing Morphology Typology Ecology Capacity

Site	Opportunity	Change	Theme
11.	Water Gardens accessible greenspace	Rezone C2 to MU1 and RE1, revisiting the Water Gardens ponds, accessible greenspace, and picnic lawn.	Flourishing Morphology Typology Ecology Capacity
12.	Old Princes Highway professional service corridor, Forestry Corporation heritage site	Construct the east west connectivity for Crown Street, connect footpath to Water Gardens shared paths. Rezone from R3 to MU1	Flourishing Morphology Typology Ecology Capacity
13.	Crown Street completion	Raise and complete the east west connection of Crown Street from Old Prince's Highway to High Street to open a third route for traffic and pedestrian permeability and movement across the town.	Flourishing Morphology Typology Ecology Capacity

Table 2 Commercial opportunities sites

DRAFT

3 Residential Quarter

The following table of residential opportunities expect a mix of developer, community and Council led initiatives that aim to improve the quality of life and architectural style of the town.



Figure 60 Residential opportunities

Site	Opportunity	Change	Theme
1.	The Hill Bent Street residential apartments	The prominence of the site offers divergent outcomes either low rise massing limited to the 20m tree canopy or a landmark tower. Increased height RL2 offers a mid-point between these two.	Morphology Typology Capacity
2.	Beach Road Boulevard	Centreline street tree planting, road raising	Morphology Typology Ecology Capacity
3.	Bavarde Avenue, Herarde Street and Heradale Parade	Increase height with RL2 a backdrop to general RL1	Morphology Typology Capacity
4.	Batemans Bay Marina	Harbour excavation of the southern end of the marina,	Flourishing Morphology Typology Capacity
5.	Batemans Bay Marina resort	Quayside redevelopment, rezoning SP 2 tourism to R3 residential, general RL1 along the foreshore with increased heights to RL2 and a 'balcony' of RL3. Public road connections to the quayside and through to Hanging Rock Place, connection to the coastal walkway, new marina sea wall rock fishing pods.	Flourishing Morphology Typology Ecology Capacity
6.	Catalina Golf Course car park and Golf links drive	Increased car park height to RL4. Increase the height of Golf Links Drive westerns side next to the golf course to RL 2.	Flourishing Morphology Typology Capacity
7.	Hanging Rock Boating hub	Relocate the yacht club and secure storage to a harbour wall setting, collocate the Marine fisheries unit and facilities to the Marine recuse site with added secure boat storage	Morphology Ecology Capacity
8.	Hanging Rock sports hub	Support accommodating a stadium and sports administration and accommodation hub rezoning from RE1 zone community land to RE1 zone operational land.	Morphology Ecology Capacity
9.	Batehaven Commercial centre	Increase height to RL2	Flourishing Morphology Typology Ecology Capacity

Table 3 Residential opportunities

4 Industrial Quarter

The following table of industrial opportunities expect a mix of developer, community and Council led initiatives.

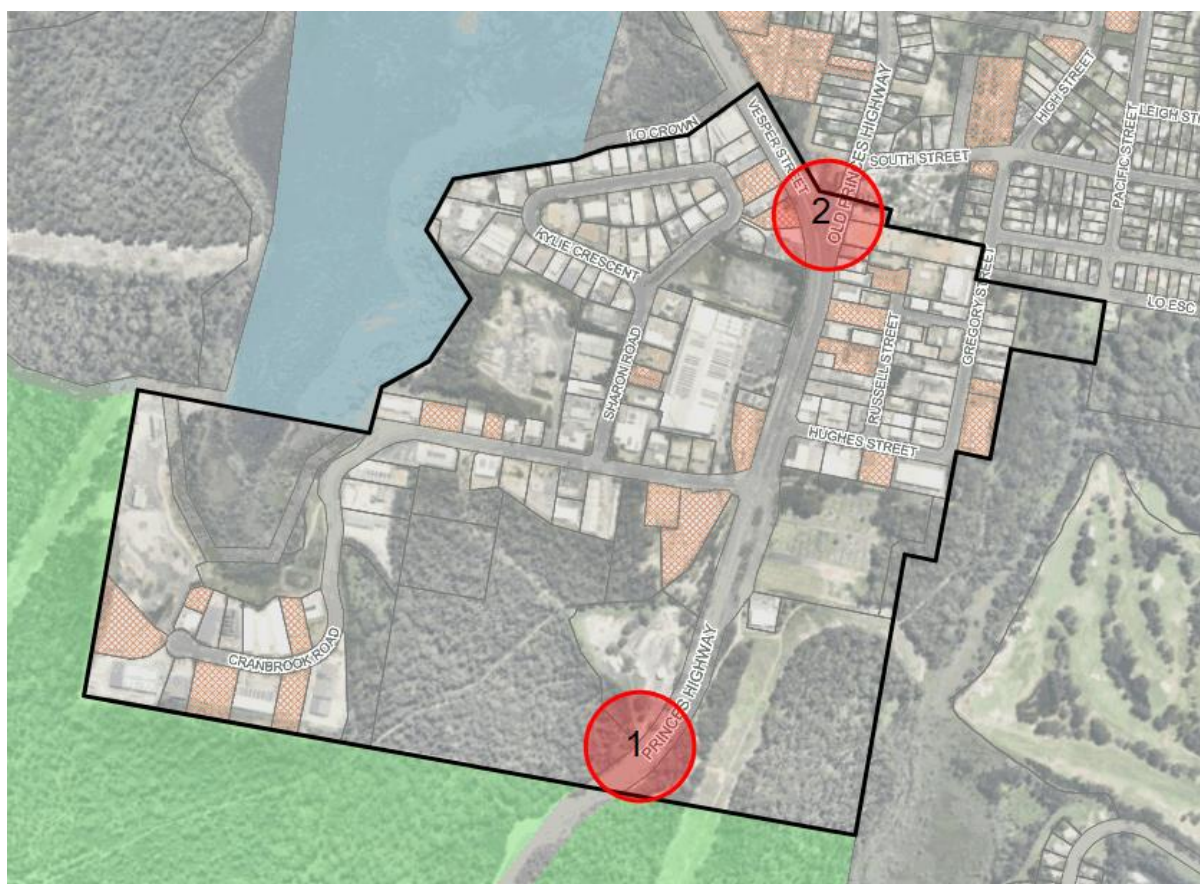


Figure 61 Industrial Quarter action sites

Site	Opportunity	Change	Theme
1.	Gateway to the Town Centre	Applying intensive landscape design to create an attractive entry point to the town.	Flourishing Morphology
2.	Intersection and Traffic Management	Rationalisation by redesigning the three-way intersection and slip road to improve clarity and accessibility and manage traffic flow.	Flourishing Morphology Capacity
3.	Industrial Land Supply Audit	Conduct and industrial land supply audit to assess current and future land needs.	Flourishing Morphology Typology Ecology Capacity
4.	Site Identification	Identifying potential sites for future industrial development to enable adequate supply.	Flourishing Morphology Typology Ecology Capacity

Table 4 Industrial Opportunities

Policy Actions

Site	Opportunity	Change	Theme
1.	Streets and Laneways	Prepare a Strategy relating to the hierarchy for streets and laneways, to beautify the spaces and provide improved landscaping. The policy needs to contain tree species, lighting and activation of the species.	Flourishing Morphology Capacity
2.	Greening	Prepare a Greening Strategy that links together the public open spaces and encourages the planting not only within road reserves, but also on the face and roof of buildings.	Ecology
3.	Celebratory Spaces	Prepare a Planning Proposal to amend Eurobodalla Local Environmental Plan 2012 to allow for events to be carried out in public reserves as exempt development.	Flourishing Typology
4.	Outdoor Refreshment Areas	Create opportunities for outdoor dining areas at street level and above street level.	Flourishing Morphology Typology
5.	Height	Prepare a planning proposal to define the height of development to align with the RL method.	Flourishing Morphology Typology
6.	Zone	Prepare a planning proposal to define the required amended land zones to support the details in the Appendix.	Flourishing Morphology Typology Ecology Capacity
7.	Development Controls	Prepare a Development Control Plan to define the types of developments as envisaged by this plan. Including: <ul style="list-style-type: none"> - Form - Fabric - Refreshment areas and Outdoor dining areas - Articulation - Sky terraces 	Flourishing Morphology Typology Ecology Capacity
8.	Design Excellence	Prepare a planning proposal to amend clause 6.14 of the Eurobodalla Local Environmental Plan 2012 to enable the design excellence requirements for Batemans Bay are consistent with the desires in this plan.	Flourishing Morphology Typology Ecology Capacity

Table 5 Policy Opportunities

Appendices

DRAFT

Appendix 1 – The Master Plan

DRAFT

Appendix 2 – Key sites ELEP 2012 height map changes

The proposed height for new buildings is shown as a reduced level (RL) in the following plans. The RL is a distance from the natural ground level for the site expressed in metres. Floors or stories in buildings vary in height depending on use and the intended desirable atmosphere. Commercial buildings may have a floor-to-ceiling height between 3.6 and 5 meters, depending on whether it is an office, retail, or theatre building. Residential building height is often closer to 3.0 metres, sometimes 2.7m, and as much as 3.6 metres for luxury apartments. Where buildings have a mixed-use with retail on the ground floor, some office or auditorium space above, and then several floors of residential, hotel, serviced apartments and standard residential uses, building floor-to-ceiling height can be assumed at about 3 meters. So, in this example, a 60m high building could have between 15 and 18 floors plus a service area with lift and air conditioning. Car parking levels might be closer to 2.7m plus 300mm for the concrete floor.

DRAFT

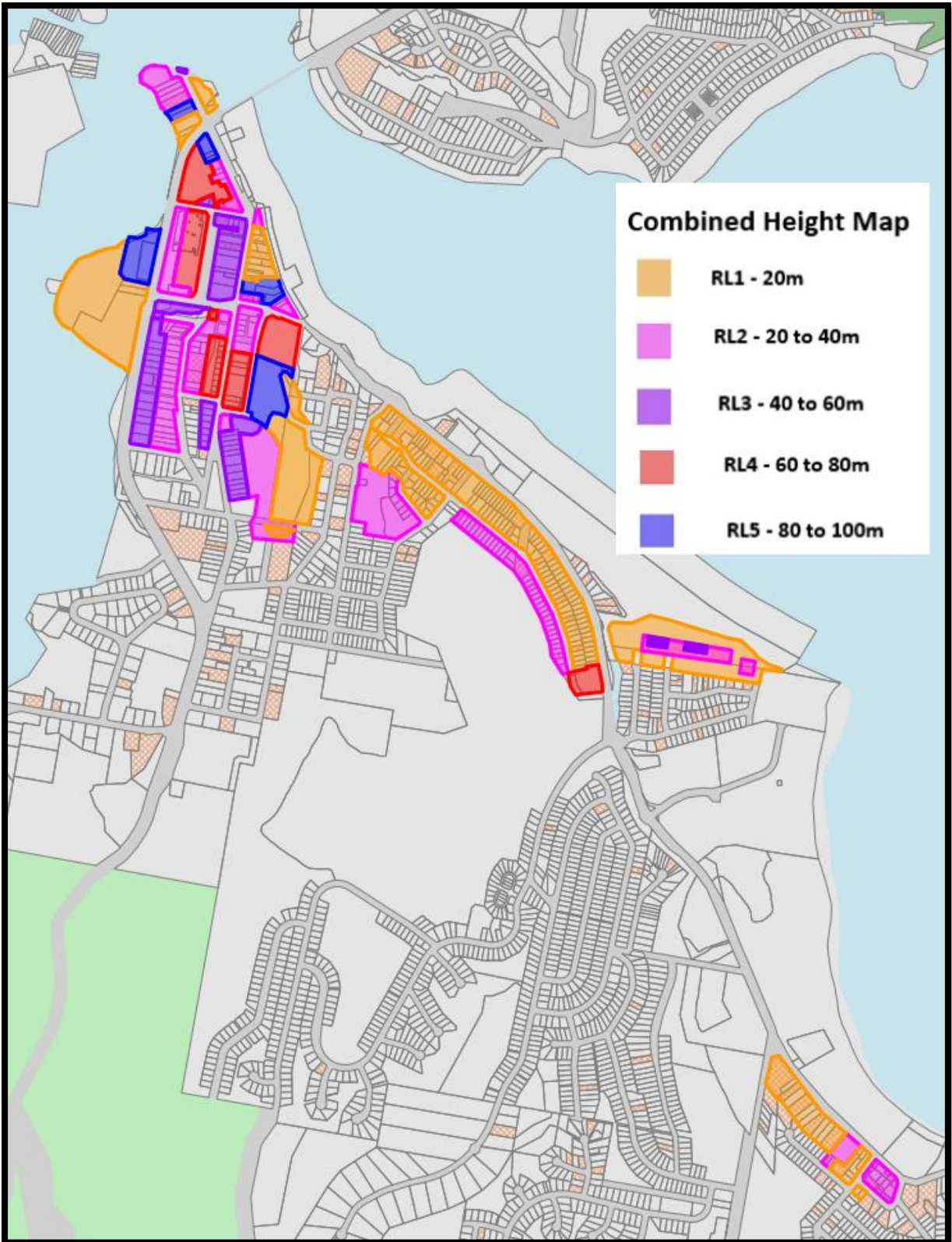


Figure 62 Combined Height Map depicting proposed changes to existing mapping



Figure 63 Map of area depicting proposed RL1 – 0 to 20m

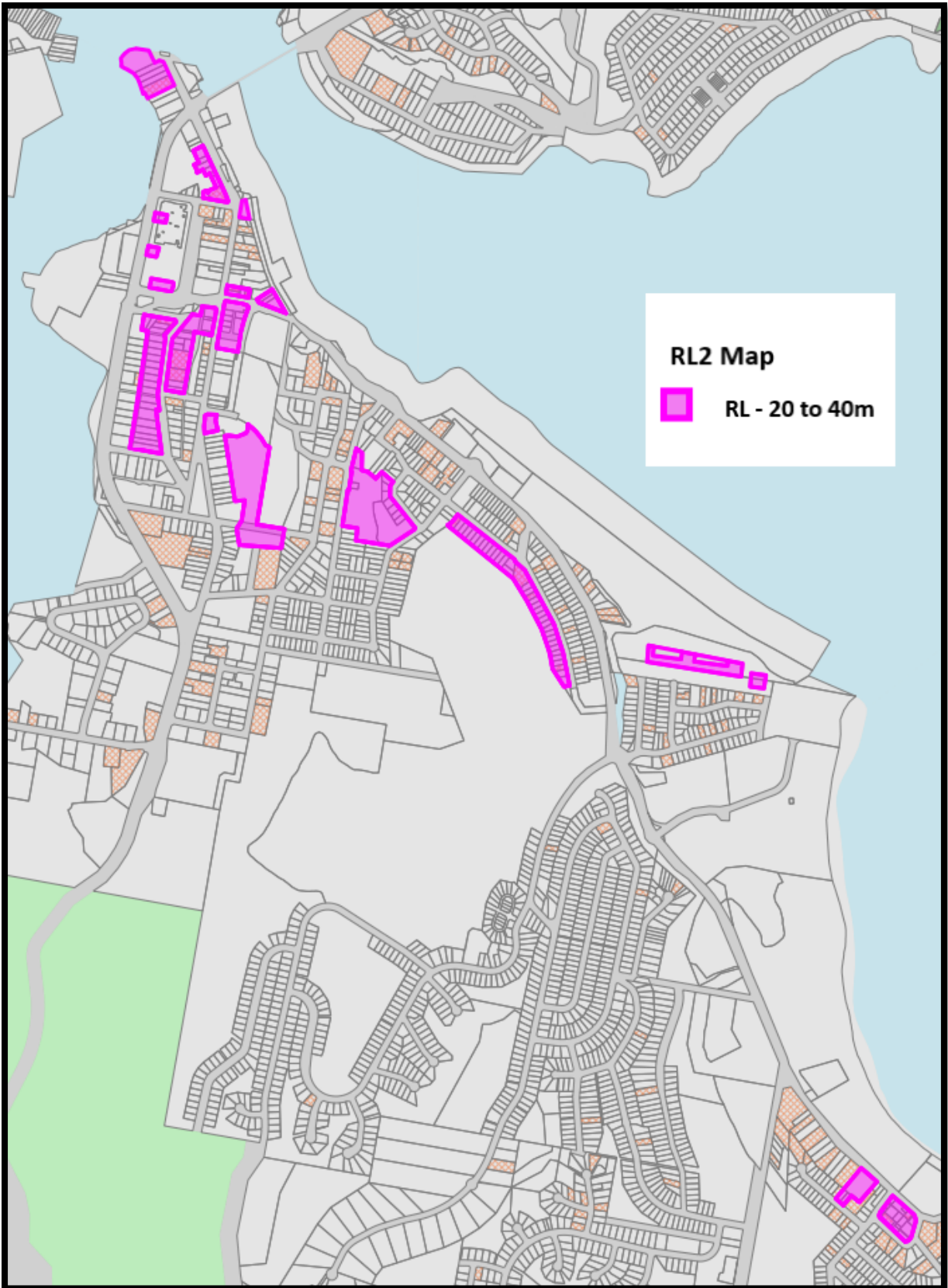


Figure 64 Map of area depicting proposed RL2 – 20 to 40m

Note: Base 20m, 5m increment incentives apply

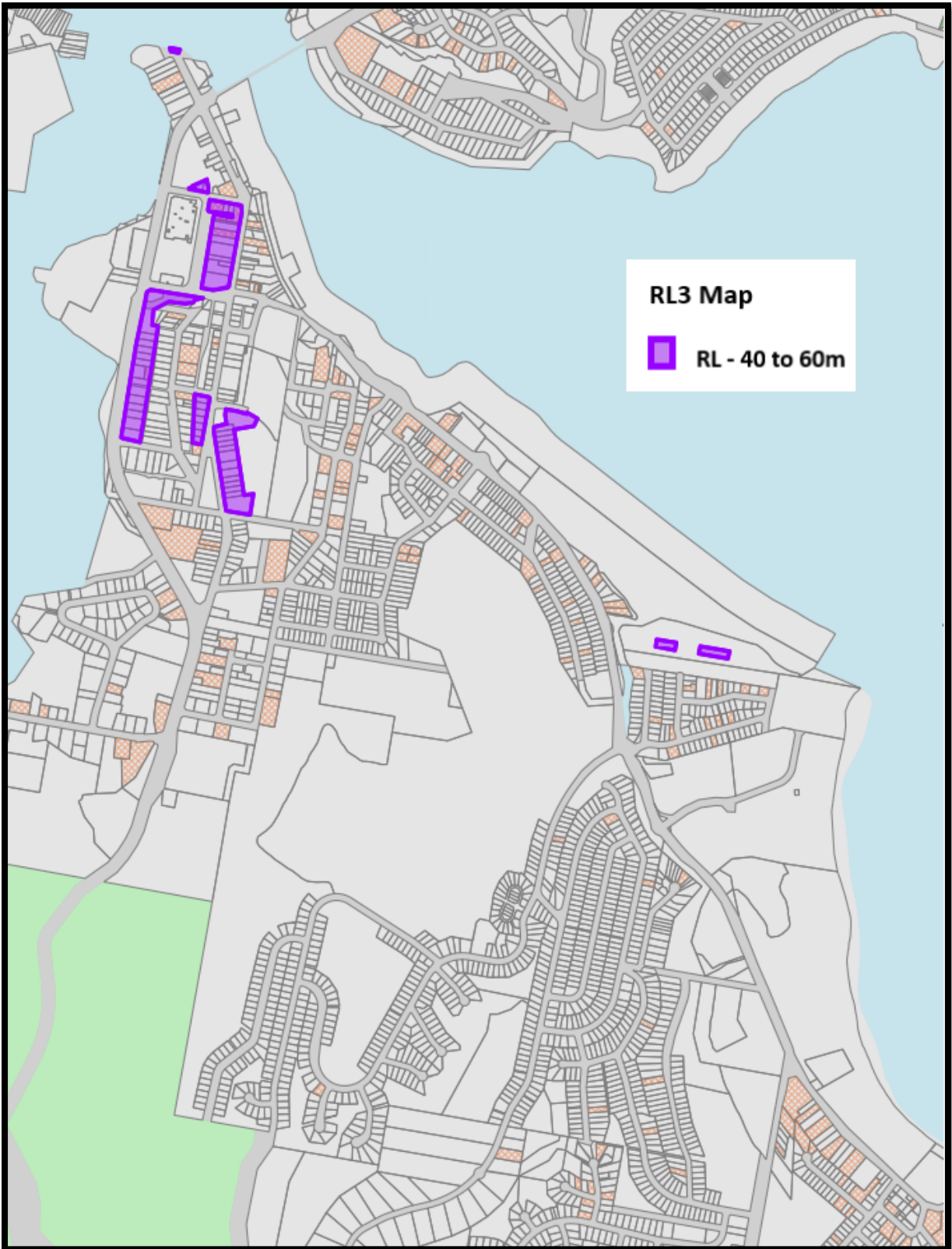


Figure 65 Map of area depicting proposed RL3 – 40 to 60m

Note: Base 40m, 5m increment incentives apply

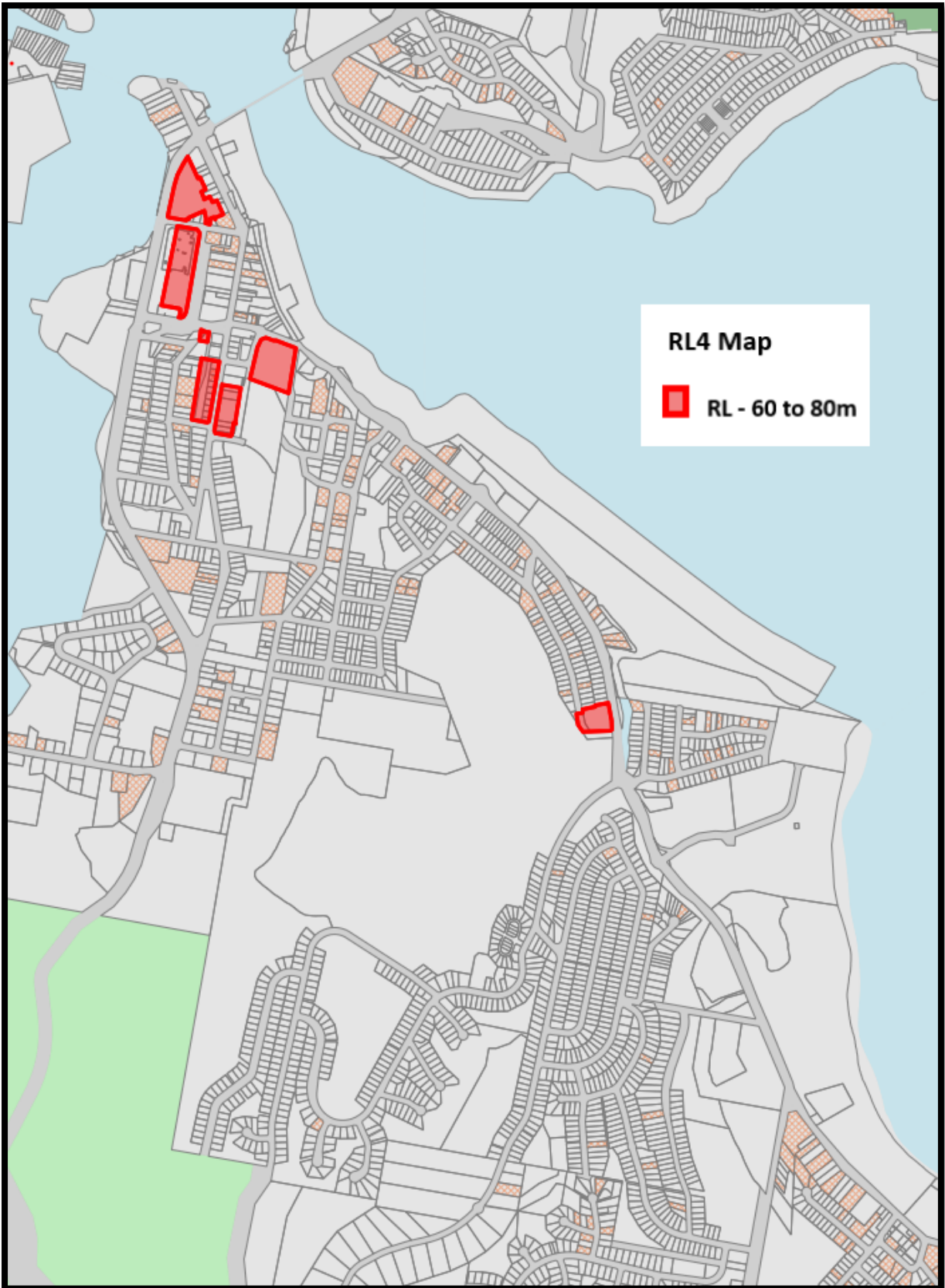


Figure 66 Map of area depicting proposed RL4 – 60 to 80m

Note: Base 60m, 5m increment incentives apply

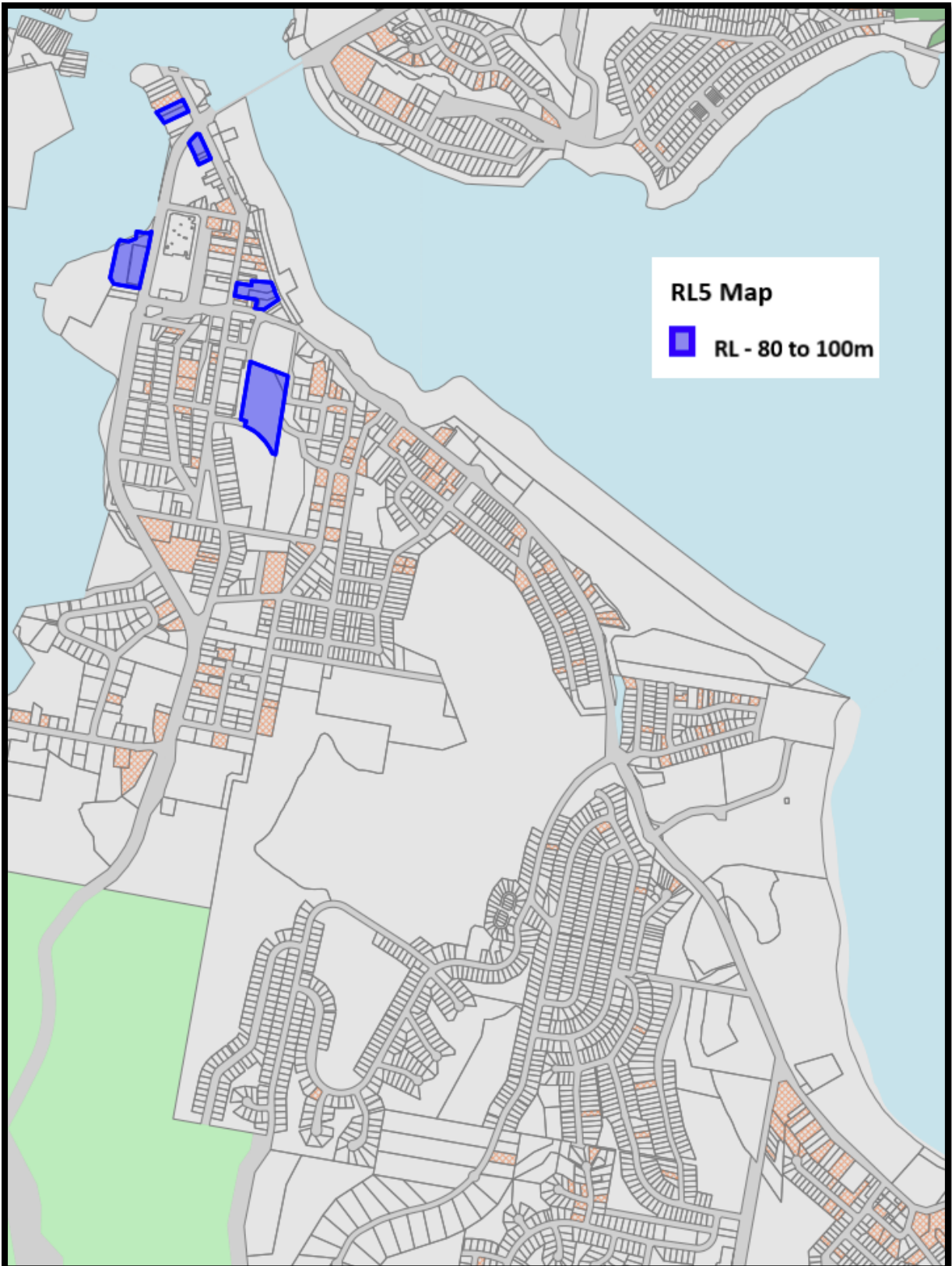


Figure 67 Map of area depicting proposed RL5 – 80 to 100m

Note: Base 80m, 5m increment incentives apply

Appendix 3 – Proposed Policy Changes

Policy Changes

The Masterplan drafting has identified the need to amend existing Council policies to adapt to the desired future character. To achieve this, the following sections outline the intended changes and new introductions to planning policies.

Heritage and Indigenous Identity

Highlight Aboriginal heritage with art installations, storytelling trails and a cultural space.

Streets and Laneways

The town has local roads and highways/State Roads. Highways and State Roads can be enhanced with tree planting, gateways, and signage. This plan emphasises greening local streets, activating lanes and pathways, and developing a network of pedestrian and cycle paths. Enhancing the streetscape with adaptive reuse of building surfaces (like exposed brickwork, stone, and timber) can improve the experience of moving through the town. Proposals include closing streets (e.g., North Street at Vesper Street) and altering traffic flow (e.g., Tourism Quarter and Perry Street). Roads can be used for markets, and parking spaces can become temporary activity spaces (e.g., market stalls or parklets). These changes aim to create interest and curiosity without disrupting essential functions.

The plan can also define multipurpose use of existing spaces, such as car parking. Parklet urban interventions allow temporary use of street space for community activities instead of car parking. These parklets act as street calming devices, making areas with high foot traffic safer by creating a sense of congestion that increases drivers' awareness of pedestrians and other road users. Potential locations for these installations include Commercial Lane, Perry Street, Flora Crescent, Corrigan Crescent, and Beach Road.

The existing street networks consisting of Orient Street and Beach Road are suitable for the installation of tree planting to provide for a boulevard experience. This can be achieved through either inground planting or the use of planter boxes. The installation of tree planting in either option is proposed to reduce the amount of radiant heat transfer that currently exists in the town centre.

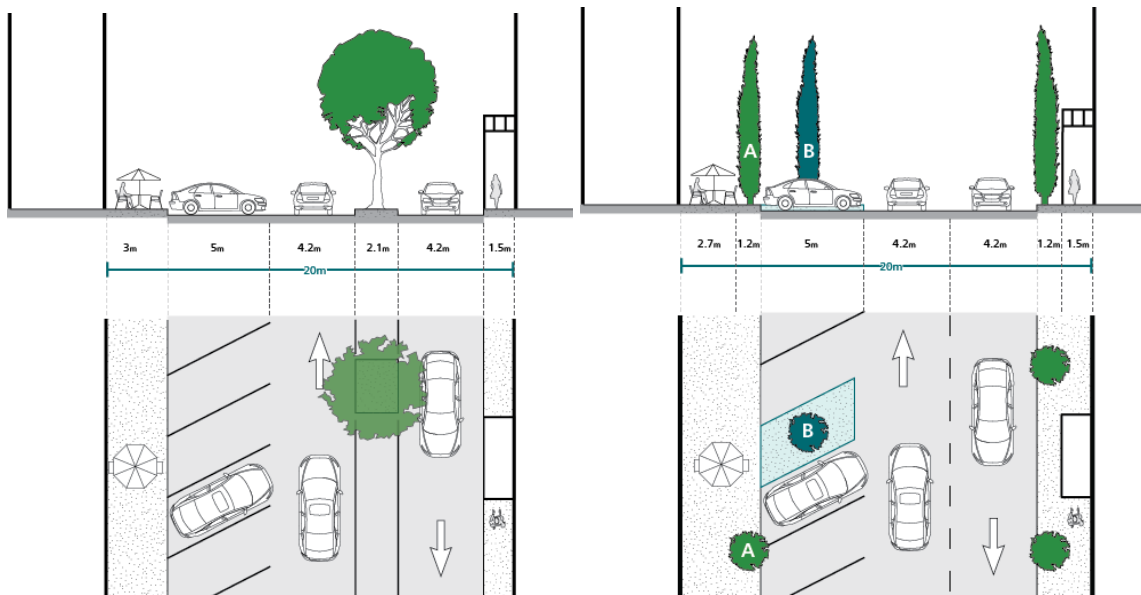


Figure 68 Indicative centreline and boulevard style tree planting and car parking options

The Perry Street and Orient Street laneways need consolidation and upgrade. Perry Street's urban green space offers an opportunity for formalized tree planting, using symmetry, colour, fragrance, and fairy lights to create an interesting space. Lighting these laneways is crucial for safety. Encouraging public street art, unique landscape interventions, "hole in the wall" retail, and "pop-up" installations and activations in these spaces is recommended.

Greening

The master plan proposes the installation of greening across the urban footprint, including both public and private spaces. Ventures include a reimagined Water Gardens, a new park on Perry Street, and a shared space network along the Clyde River and foreshore.

This includes landscaping at ground level, on buildings and above the streetscape. This can include outdoor dining with landscaping, communal rooftop pavilions, and sky terraces open to the public.

The masterplan includes incentives for the installation of green walls, rooftop landscaping. Developments can count features like Sky Terraces, Communal Planter Boxes, and Covered Communal Ground Gardens towards the landscape requirement.

At least 40% of this must be softscape (permanent planting), with the rest as hardscape. The Green Plot Ratio (GPR) measures greenery density, calculated as the total canopy area divided by the site area.

- Location site area (LSA): 100%
- Minimum softs landscaping: 40%

This approach could be part of development control monitoring.

Street trees

Deciduous street trees are preferred for their winter sun access, fragrance, and colour. They also provide sculptural forms that can be highlighted with uplighting or tree lights. Tree selection should consider colour, form, and overall experience, focusing on form, consistency, uniformity, placement, seasonal features, and water-sensitive urban design.

Avoid using multiple tree species to prevent a piecemeal effect. While many exotic species are bred for urban environments, most native species are unsuitable due to bushfire risks, limb dropping, and other issues. Trees providing a canopy form is preferred. This provides shade and reduces radiant heat.

Street tree placement should prioritise infrastructure upgrades and new infrastructure plans, with a preference for centre median trees for greater canopy cover. Alternatives like planters and trellis structures can be used where ground conditions do not allow for plantings. All trees and plants installed should be fit for purpose.

Night lighting of street trees creates visual interest and vibrancy, focusing attention on main retail or tourism connections. A consistent approach to tree lighting reinforces movement and safety, with uplighting and fairy lights being effective options. Tree lighting also serves as an important wayfinding tool.

Celebratory Spaces

The council's public reserves and public spaces are to be promoted as public event spaces. Each event requires a Development Application as the events are not exempt. To encourage events and activation of our public spaces, it is proposed to include a new exempt development in Schedule 2 of the Eurobodalla Local Environmental Plan 2012. This will require a planning proposal to include the use as exempt development.

Outdoor Refreshment Areas

Outdoor Refreshment Areas are above-street dining spots on the first storey, offering views of the coast and inland. They front the street and can be in courtyards or open plazas, ideally on private development sites that enhance nearby laneways or pedestrian areas. These spaces create a sense of enclosure with landscaping, unique design, public art, and vertical green walls, while remaining open and safe.

The scale and design should complement adjacent buildings and include "hole in the wall" retail. Developers should incorporate these areas at the design stage, ensuring seamless transitions from interior to exterior spaces without encroaching on pedestrian pathways, using different floor finishes or planters to define boundaries.

Communal Pavilions

Communal pavilions are public open spaces located on rooftops. They count as GFA unless they meet the following criteria:

- a. Maximum 50m² or 50% of roof coverage, whichever is lower.
- b. For developments with storey height control, pavilions must be open-sided and unroofed.
- c. For developments without storey height control, up to 50% perimeter enclosure is permitted.
- d. Pavilions must be integrated with lush greenery, enjoyable for users and visible from the surroundings.
- e. Plants should be in permanent, preferably sunken, planters with sufficient soil depth. Space must be set aside for communal facilities and furniture to enhance the rooftop.

Managing form

Traditional architectural forms have evolved significantly, in the last century with the rise of modernism and box-like structures that lack ornamentation. However, there is a renewed

appreciation for classical design values, leading to a resurgence of natural curves and decorative elements that are viewed in contemporary architecture. This shift has changed how developers view projects, increasingly seeing them as expressions of brand identity.

The existing hills and valleys support a varied skyline. Instead of constructing uniform-height buildings along the waterfront — the proposed "amphitheatre" strategy suggests that building heights should increase as one moves away from the coast. This will allow increased and improved view sharing and diversity in height.

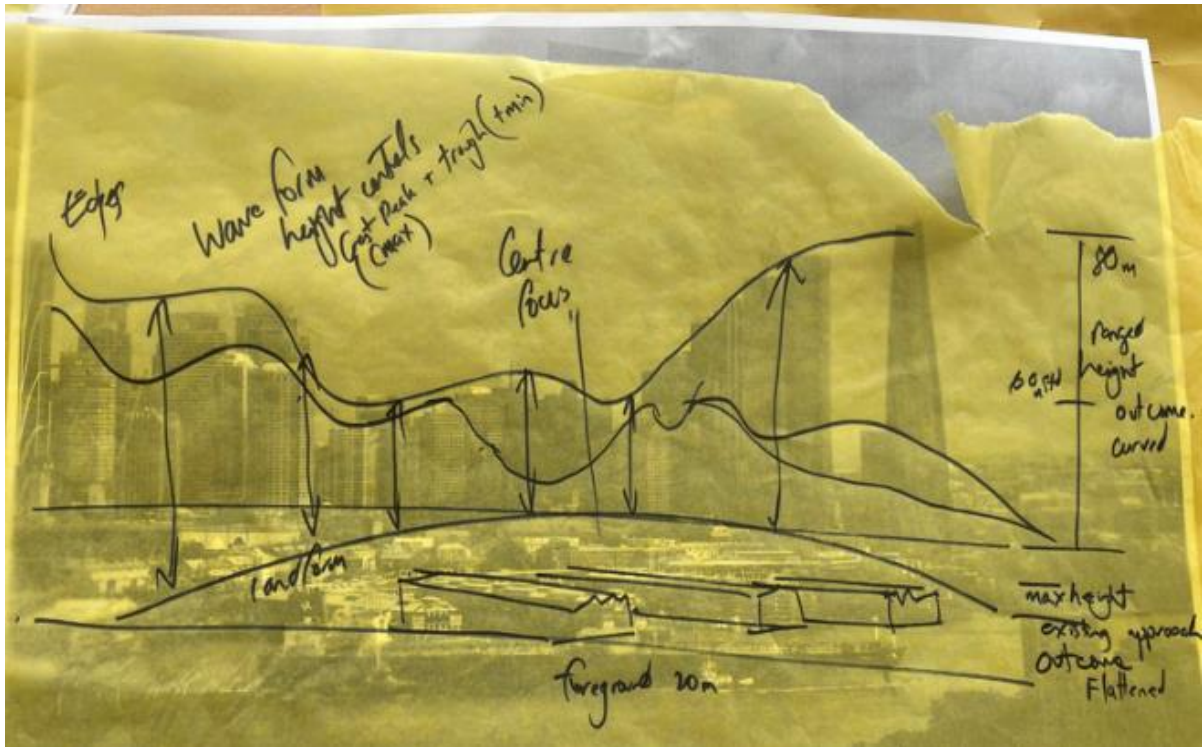


Figure 69 Wave Form Approach to the skyline

In utilising amphitheatre style development, this enables visual interest to be achieved in the built form. It is important to establish sightlines and height references in this masterplan. This is achieved through identifying key sites and mapping the various heights across the study area.

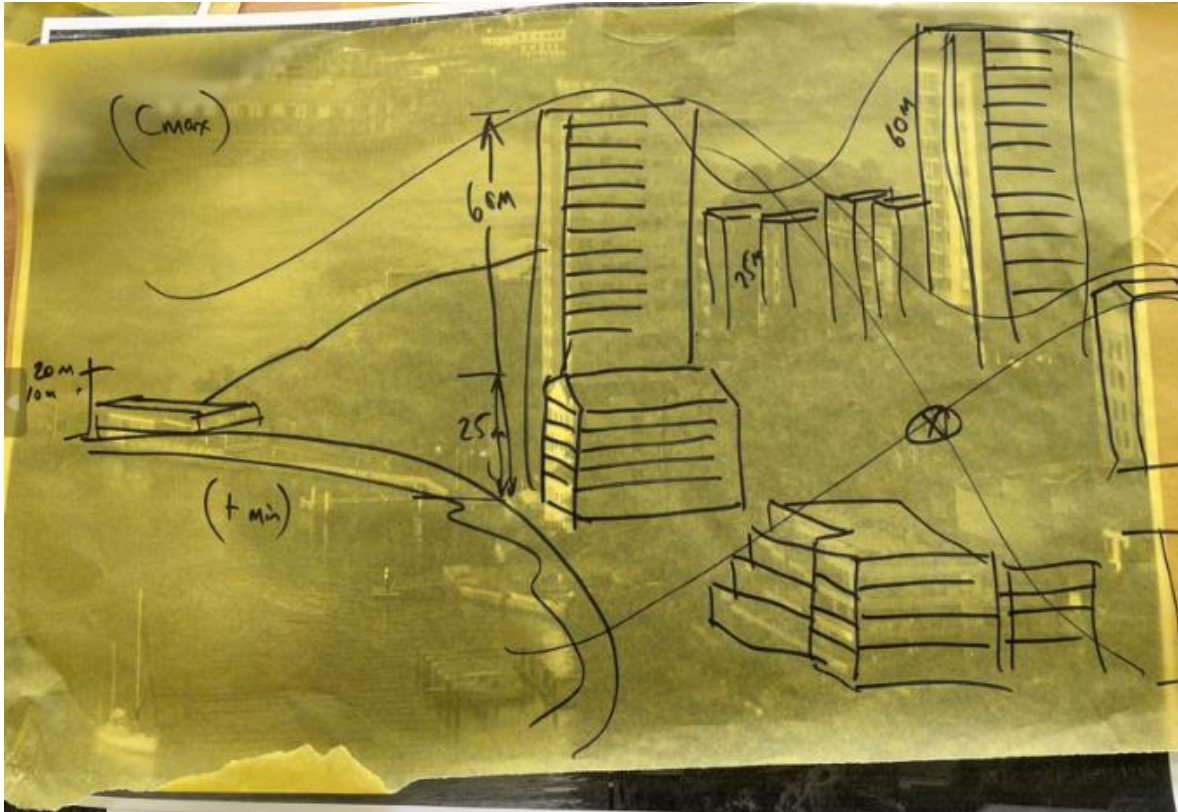


Figure 70 Amphitheatre style of construction heights

The identification of Gateway sites are crucial to the town's development. These sites should be symmetrical. Each can vary in height while contributing to a cohesive overall design. If the first building at a gateway reaches a specific height, it sets a standard for future developments, ensuring visual harmony.

This plan also identifies other key sites where it is proposed to alter the height. The aim is to harmonise the height with the existing urban fabric whilst ensuring the sites are distinct. The design process must prioritise visual cohesion, ensuring new buildings contain interesting designs which enhance the amenity of the town.

Design Excellence

To guide architectural design innovation towards beauty and iconic outcomes, the Tourism Quarter landmark sites will attract specific design attention from the Council and require compliance with design excellence clause 6.14 of the ELP 2012. Designs will demonstrate sculptural form, urban design, and interior design involvement in the exterior ornamentation of the building.

Council will amend Clause 6.14 Sub clause (1) to also apply to any development in height locations utilizing RL3, RL4, and RL5.

In addition, subclause (b) *whether the form and external appearance of the development will improve the quality and amenity of the public domain* will be amended to add 'beauty'.

A new subclause to 6.14 (d) will *classical architecture design principles and the use of natural materials, colour, exterior lighting, sculptural form, artisan ornamentation, and external application of interior design to adjacent public space.*

These requirements aim to enable design creativity and facilitate natural beauty in architectural design.

Design focus is on creating a form that would complement rather than compete – creating a companion building, rather than something separate and unrelated.

Urban Form and Fabric Incentives

The goal of managing development capacity is to both enable community benefits and encourage developers to provide more than just basic structures. Key incentives include offering new housing types and affordable housing in Batemans Bay.

Adding public spaces above street level, such as outdoor dining, sky plazas, and rooftop gardens, can enhance community benefits and attract developers. An incentive scheme should be established to introduce compensatory benefits. Developers should cover the design and construction costs for adjacent public spaces, not the Council.

Increasing floorspace requires re-evaluating height limits. A flexible sliding scale for height offsets can provide benefits without strict limits

Natural Forms

The concept of wave formation as a height control creates a dynamic skyline with building heights that ebb and flow. Higher elements crest at key locations, while lower elements step down between them.

No two buildings should be the same height, allowing for varied vistas and preserving views of the ocean, hills, and ridgelines.

Incorporating natural elements from the coastal setting into architectural design helps frame the town's transformation into a beautiful space that preserves and respects our natural environment. Redevelopment of key sites should draw inspiration from the local context of water, land, air, flora, and fauna in Batemans Bay and the Clyde River. The design rationale must be provided during the development application phase.

General design policy

The following policies draw on those classical design inspirations and achieve excellence in design.

- Roof areas are to be well-designed and attractive when viewed from the surrounding developments.
 - For flat roofs, roof parapet walls may be built to a maximum height of 1m. However, architectural features on such walls may exceed this limit and will be evaluated on a case by-case basis.
- The development is to be built up to the common boundary abutting to the adjacent building with a party wall built to the full height of the development. Any party wall along this common boundary that is exposed as an external wall shall not have any openings.
- The height map outlines a range of heights for the built form. This range in base height is then supported by incentives. The incentives allow for an increased height, depending on compliance with the incentives. Each incentive allows for an additional 5m in height increments, totalling an additional 20m.

Four incentives to be implemented in this plan:

- Greening incentives: Increased development rights where buildings provide public benefit green space above the street level.
- Affordability incentives: A developer contracts with a social housing provider to provide affordable rental housing within a development. Affordable housing units will be scattered throughout the development.
- Height incentives: A height range offered for certain sites, which is scattered across the study area. The incentives will only be triggered when the incentives are met. Please refer to RL maps in the Appendix.
- Design Incentives: Buildings must integrate into the streetscape providing creative, innovative and beautiful design. Nature-based inspiration must be integrated into the design, external colours and finishes, and lighting. It is recommended that a design review panel be established, consisting of professionals with extensive experience in architecture, urban design and interior design. The style of architecture should relate to and draw inspiration from nature and the local context.

Building and Mass

The overall building form and massing should consider the scale, form, and architectural style of surrounding buildings. Designs should enhance the town's skyline and pedestrian experience. The building's form, massing, and architectural treatment should be viewed positively from the main axis through town, Surfside, major approaches, key open spaces, and shared pathways. Additionally, shopfronts along main pedestrian routes should be engaging and varied.

Articulation

Street frontage articulation can be achieved through the following:

- a. Up to 40% of the building frontage can be setback from the setback line to create varied spaces and interesting fronts.
- b. Up to 50% of the podium facade can project within the setback as Façade articulation for more varied frontages.
- c. Multi-storied shop fronts with high transparency and attractive lighting are encouraged to add vibrancy to the streetscape.
- d. Developments along Perry and Orient Streets should include public open spaces to support nighttime street activities.

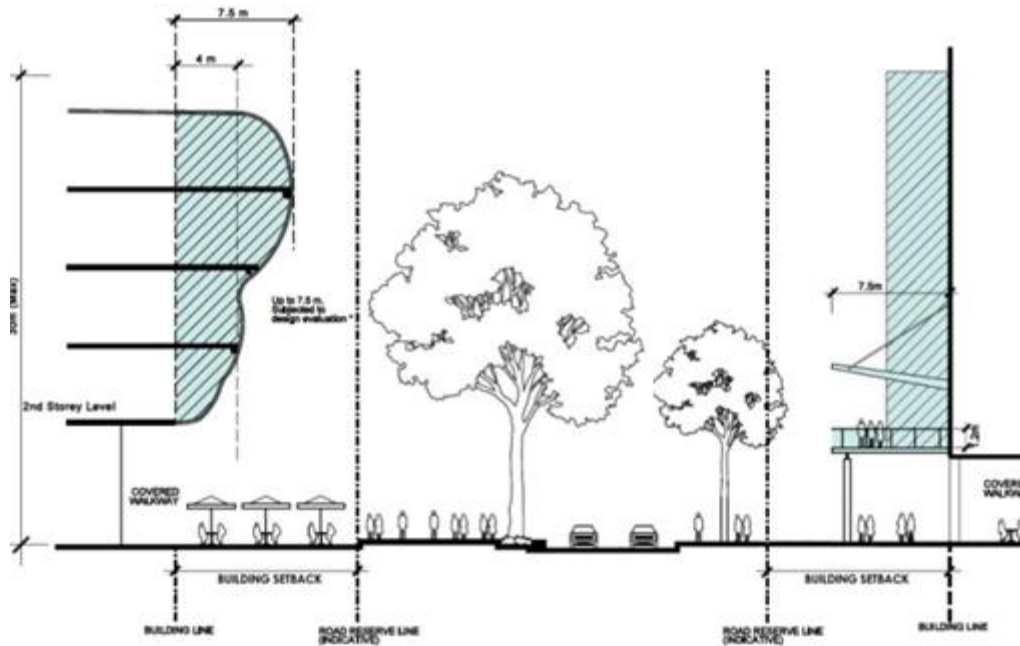


Figure 71 Façade articulations are allowed to project into the building setback

Outdoor Dining Areas

Outdoor Dining Areas can be allowed in public areas, including roadways and footpaths or on open spaces within development sites, subject to the following:

- a. Developers are encouraged to incorporate outdoor dining areas at the design stage as part of the design review height incentive.
- b. Outdoor dining areas must be clearly defined to prevent encroachment onto adjacent pedestrian thoroughfares, using planters or other physical markers to infer boundaries and guide movement.

Sky Terraces

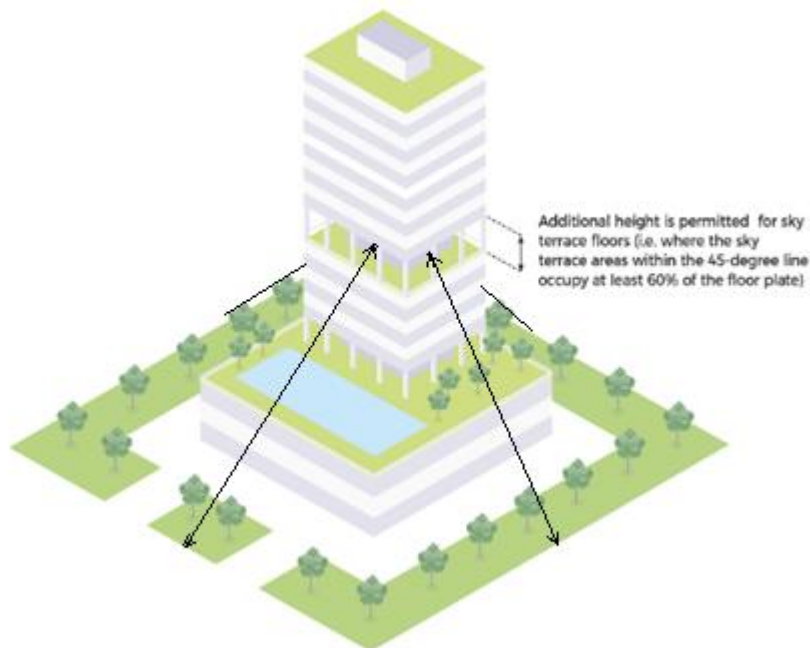


Figure 72 The sky terrace

A sky terrace is an open rooftop area that serves as a garden space, social room, and outdoor space. It is not a communal pavilion. Sky Terraces are:

- a. Areas within the 45-degree line that occupy at least 60% of the floor plate. The 45-degree boundary control enables these floors to be established below the building's mid-height while maintaining a visual connection to the streetscape.
- b. For developments containing 7 to 19 storeys with Sky terraces can use the design review incentive for an additional 5m in height.
- c. For developments containing 15 to 21 storeys with Sky terraces an additional 10m in height is available.

The added height incentive does not apply to:

- Developments within the foreshore area and Murra Murra Mia Walkway, where incentives focus on movements between the street and second floor only.
- Developments with a height control of 6 storeys or less.
- Developments with special controls, such as areas subject to street revitalisation or urban design plans.

Land Zone and Land Use Table changes

The study area contains a variety of land use zones in the Eurobodalla Local Environmental Plan 2012. A detailed review of the zones and land use tables will be undertaken as part of the Planning Proposal.

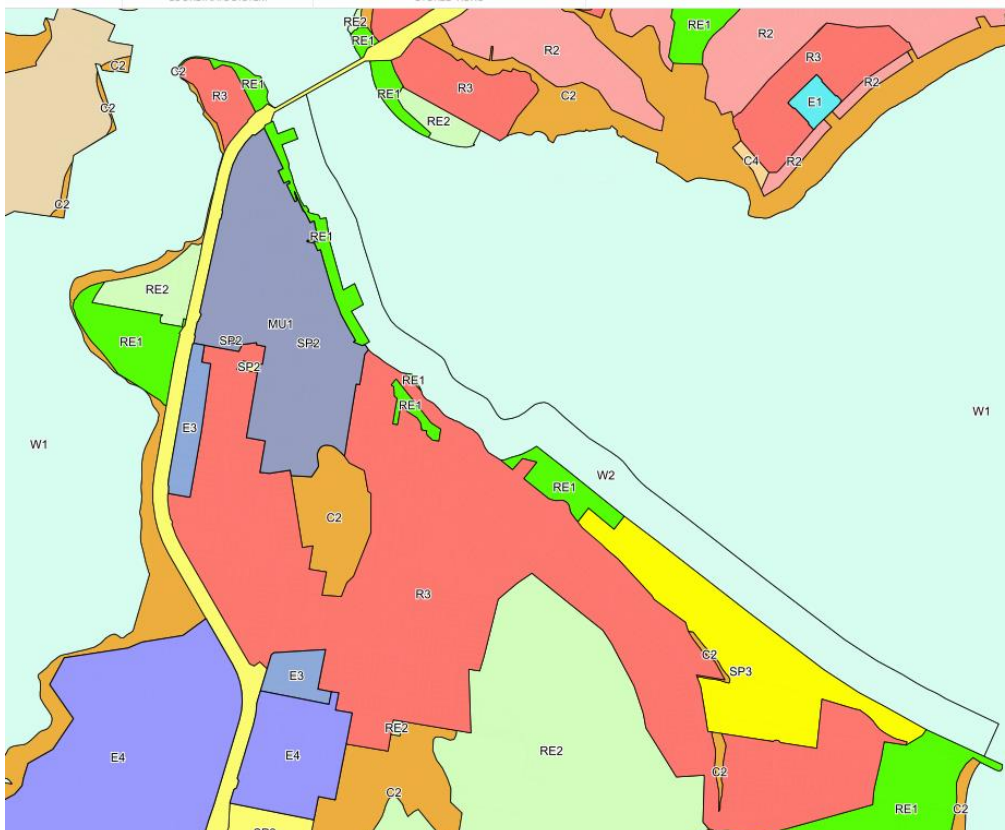


Figure 73 Current Land zone map in ELEP 2012

The current zones support many proposed land uses. To transform the CBD into distinct tourism and commercial areas, the MU1 Mixed Use zone land use table will need to be

amended to enable the Tourism Quarter contains the range of land uses necessary for establishing and realising the Tourism Quarter. The MU1 Mixed Use zone will be extended to include Smoke Point.

The W1 Natural Waterway zone will change to W2 Recreational Waterways to enable the appropriate land uses and controls are applied to developments.



Figure 74 Tourism Quarter current and future zoning

Current zones support many proposed uses. The SP2 Special Purposes zone of a driveway from Flora Crescent will change to be consistent with the surrounding land use zone of MU1 Use zone.

Part of the Water Gardens C2 Environmental Conservation zone will be changed to RE1 Public Recreation to allow recreation uses. The transition from residential to commercial services along Old Princes Highway will be supported by changing the zone from R3 Medium Density Residential to MU1 Mixed Use zone. The Forestry Corporation site will also change from R3 Medium Density Residential to MU1 Mixed Use, with possible future extension to South Street.

The ELEP 2012 design excellence clause 6.14 will apply to the Tourism and Commercial quarter MU1 zoning and key sites with RL3, RL4, or RL5 height control. Development will consider the structure plan for RL placements (see Figure 11 CBD structure).

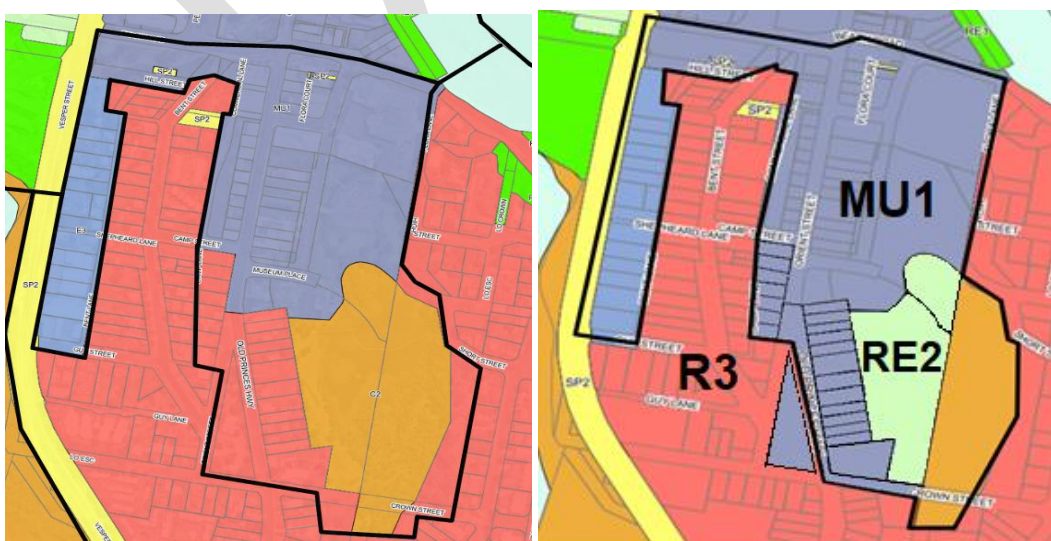


Figure 75 Current and future zoning

It is also proposed to convert the existing SP3 Tourism zone from the land at the southern end of the Marina to R3 Medium Density Residential. This will allow for a quayside style of development, activating the waters edge and containing shop top housing along a promenade.



Figure 76 Marina Zone changes

Height changes

Building heights are generally higher on the gateway and key sites, as nominated in each quarter. The heights are proposed to provide a base height with incentives for the potential to increase the base height by a further 20m. The base height represents a range in the heights to enable the wave form can be created across the landscape.

This plan recommends a changing rationale for height limits for the study area. The changes will require a formal planning proposal to amend the Height of Buildings Map and clause 4.3.

Height considerations include:

- a. Avoiding narrow, out-of-context sharp height increases ("upright pencil" effect) by scaling up and down (wave form approach) relative to adjacent buildings.
- b. Backdrop wall height next to Vesper Street/Princes Highway should echo the horizon ridgeline above the existing 6-storey buildings. Landmark sites above 60m are allowed through the ridgeline contour.
- c. Pinnacle sites should not have the same RL height, regardless of ground level.
- d. Bent Street should be lower than either end of the Tourism quarter when viewed from the bay bridge approach.

The changes nominated in this Master Plan will require a formal amendment to the Eurobodalla Local Environmental Plan 2012.

The planning proposal will need to establish the ranges now proposed in this plan. The plan proposes a range of heights within 20m bands. The bands for RL2 to RL5, inclusive, permits developers to implement developments with the additional 5m per incentive up to a maximum of 20m.

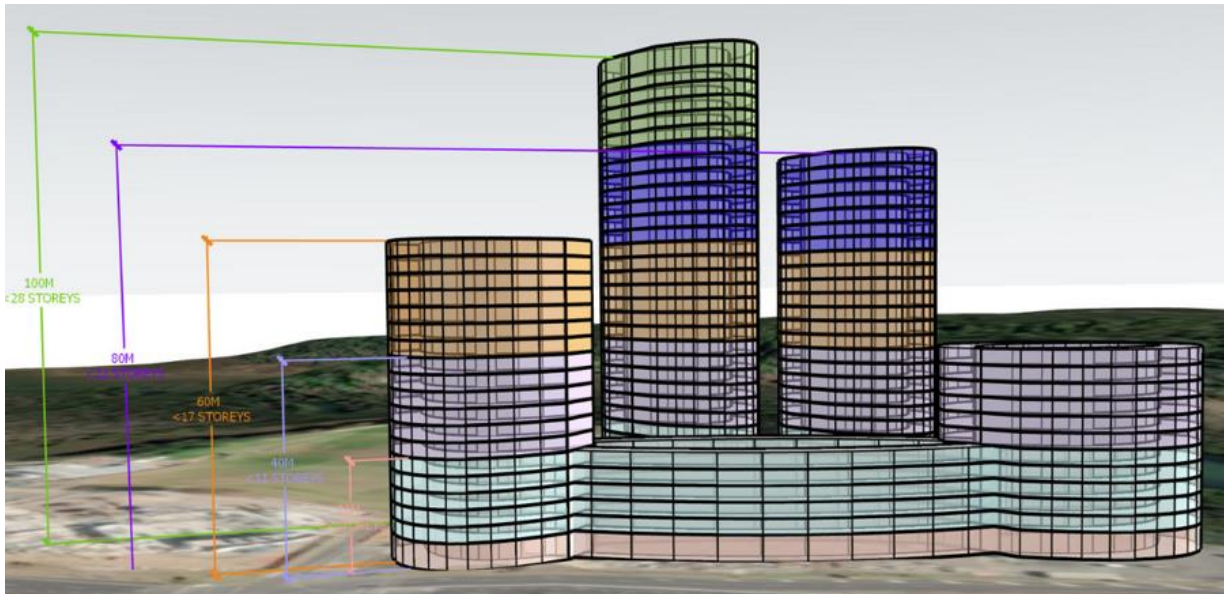


Figure 77 Example of the composition heights as a built-form outcome

Along with the anticipated height changes, complimentary design controls that prioritise beauty in the urban form will be applied through changes to Clause 6.14 of the Eurobodalla Local Environmental Plan 2012. The RL height maps are in Appendix 5.

Maximum Building Height (m)				m(RL)									
A	0 - 3.6	H	7.5 - 7.9	O	15 - 16.9	V	35 - 39.9	AC	100 - 124.9	RL1	0 - 20		
B	3.7 - 4.9	I	8 - 8.9	P	17 - 18.9	W	40 - 44.9	AD	125 - 149.9	AD	125 - 149.9	RL2	20 - 40
C	5 - 5.4	J	9 - 9.9	Q	19 - 20.9	X	45 - 49.9	AE	150 - 174.9	AE	150 - 174.9	RL3	40 - 60
D	5.5 - 5.9	K	10 - 10.9	R	21 - 22.9	Y	50 - 54.9	AF	175 - 199.9	AF	175 - 199.9	RL4	60 - 80
E	6 - 6.4	L	11 - 11.9	S	23 - 24.9	Z	55 - 59.9	AG	200 - 224.9	AG	200 - 224.9	RL5	80 - 100
F	6.5 - 6.9	M	12 - 12.9	T	25 - 29.9	AA	60 - 79.9	AH	225 - 249.9	AH	225 - 249.9	RL6	100 +
G	7 - 7.4	N	13 - 14.9	U	30 - 34.9	AB	80 - 99.9	AI	250+	AI	250+		

Figure 78 LEP2012 standard instrument heights

The Standard Instrument Local Environmental Plan allows for the establishment of a maximum height, in metres or the range using RLs. The main change in this plan is to convert the from a maximum height in metres to a range based on the RLs for specific sites as identified in the Appendices. In utilising the range, the 20m is only achieved by the developer choosing to comply with one of the four incentives. The incentives model sets parameters that tie the benefits of increasing height to providing a community benefit.

The master plan proposes adopting the RL method to create variety and interest in the urban form and meet the objective of creating a beautiful town with enduring iconic landmarks that enhance the experience of the natural beauty of its location. The base height is the minimum of each 20m band, with the variation established in the application of the additional incentives. The incentives allow for the introduction of designer-led creativity into the urban centre and built form. The creativity stems from the flexibility of height and choice of incentives allowing a design response to the uniqueness of the locality whilst aligning with more traditional land use controls.

Criteria for increases to the base maximum height ¹		e.g. RL2 (20-40m)
Community benefit	Stackable development benefit	Total maximum height = 20m
Affordable housing	+5m or 1 storey	+ 1 incentive = 25m
Greening building exterior	+5m or 1 storey	+2 incentives = 30m
ESC design review panel ²	+5m or 1 storey	+3 incentives = 35m
Applicant design group ³	+5m or 1 storey	all incentives = 40m

Table 6 Height variation incentives for movement within each m(RL) 20m band

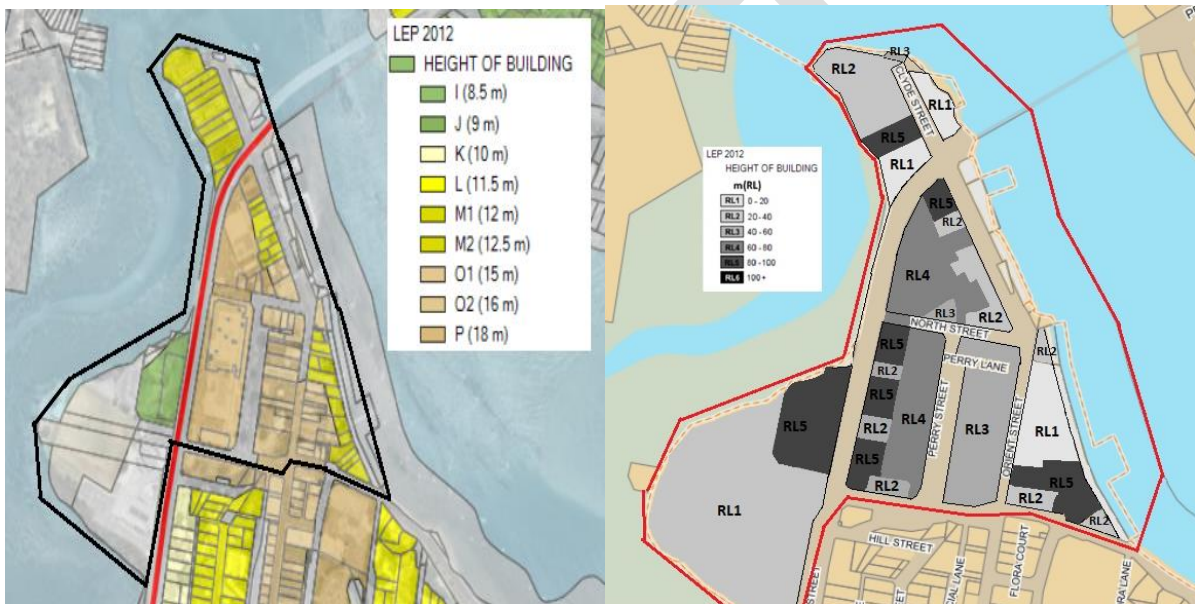


Figure 79 Current and future heights Tourism Quarter

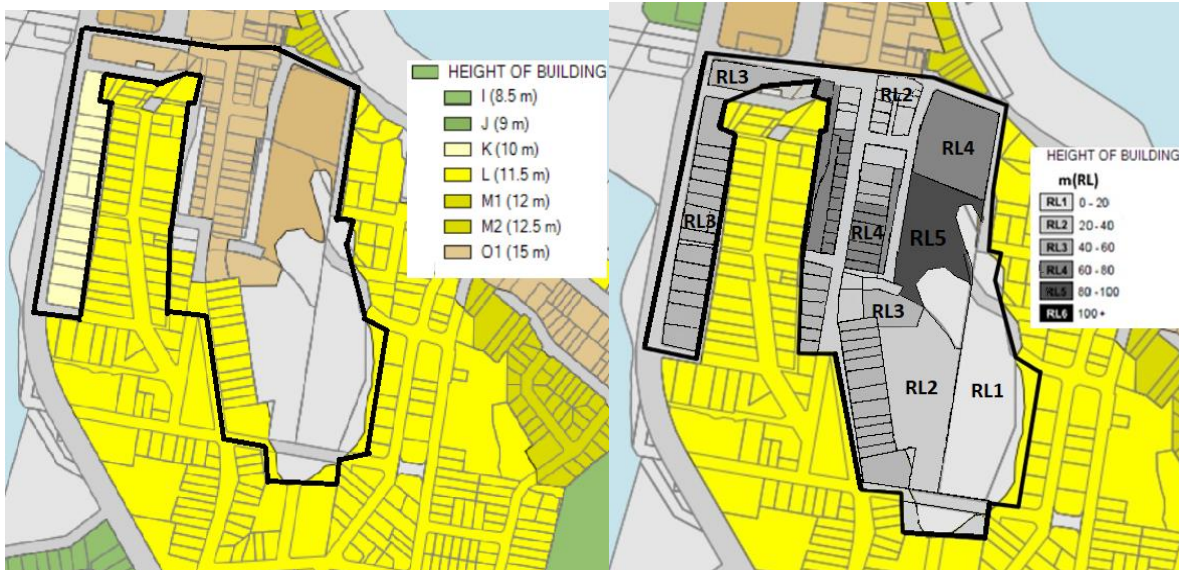


Figure 80 Current and future height Commercial Quarter

Height limits will change to a range of metres to encourage sculptural buildings, affordable housing, smaller units, green buildings, and shared recreational spaces (see Part 5). This change will also trigger redevelopment of community and cultural assets around the town park and Water Gardens. Concepts include a youth indigenous artists gallery, reflection gardens, museum spaces, and food services.

Current residential zones support many proposed uses, but height limits will change to a range of metres. The design excellence clause will apply to Residential quarter key sites with RL3, RL4, or RL5 height control, considering the structure plan (see Figure 12 Marina structure plan).

The Marina residential key site zoning remains unchanged, but the Marina resort area will have the same height flexibility and incentives as the Tourism and Commercial Quarters. The Yacht club, DPI fisheries protection site, and marine rescue building are on public community land, needing retrospective adjustments for future opportunities.

Site 1 The Hill

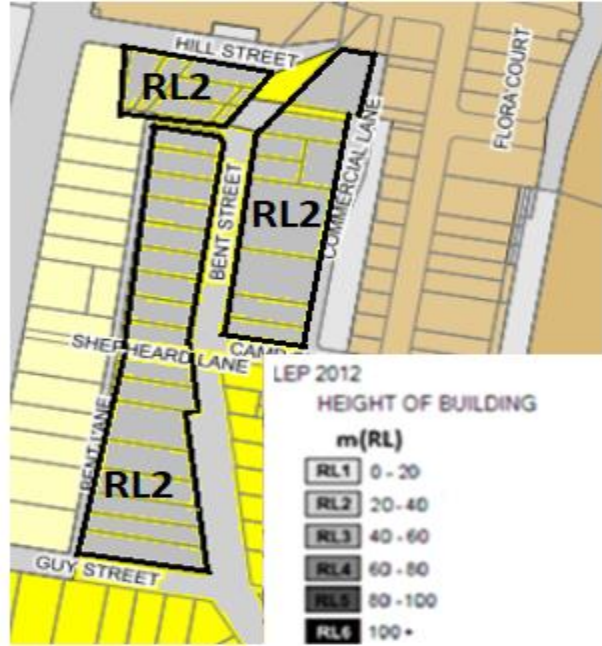


Figure 81 Height changes Bent Street

Site 2 Bavarde Street

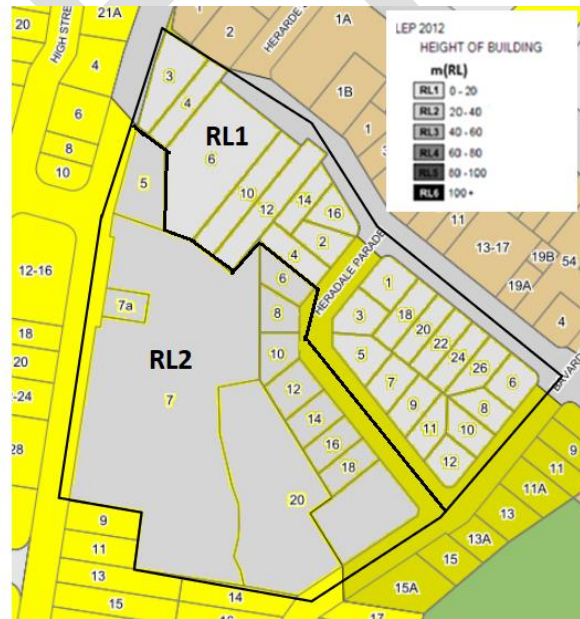
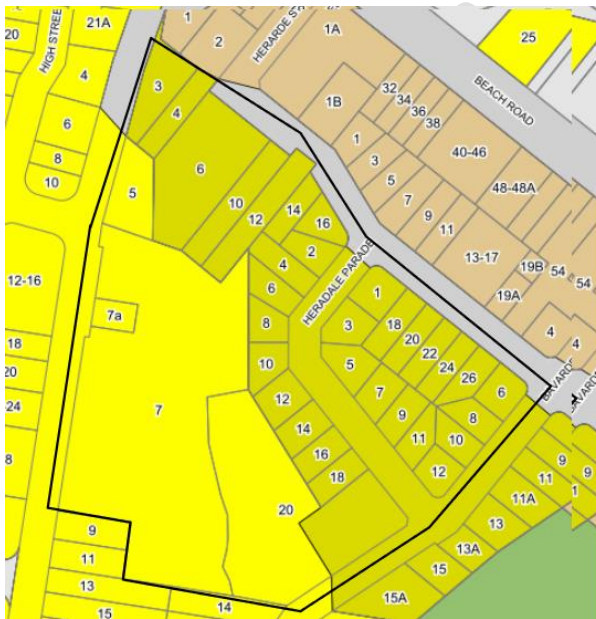


Figure 82 Height changes Bavarde Street

Site 3 Batemans Bay Marina Resort

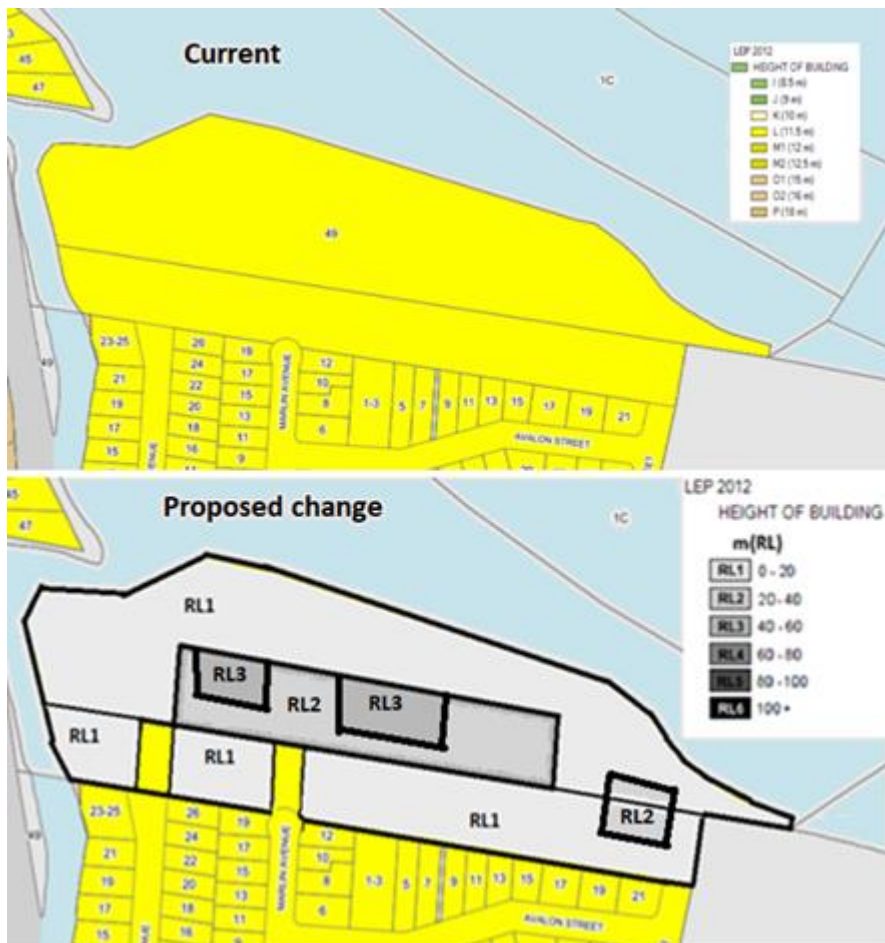


Figure 83 Height changes Batemans Bay Marina Resort

Site 10 Catalina Golf Course

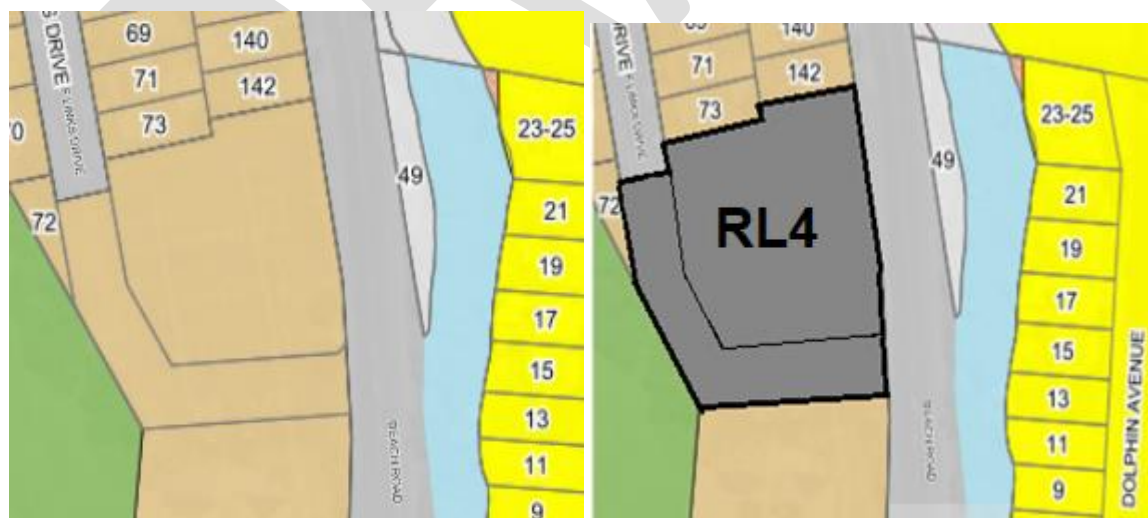


Figure 84 Height changes Catalina Golf Course

While there is no need to change the zoning an increase in height to RL4 with the range 60-80m in will be subject to the height variation incentives (see Table 2).

Eurobodalla Shire Council

Corner Vulcan and Campbell Streets, Moruya

PO Box 99

Moruya NSW 2537

02 4474 1000

council@esc.nsw.gov.au

www.esc.nsw.gov.au

Follow us on Facebook or Instagram

 www.facebook.com/EurobodallaCouncil

 [@eurobodallacouncil](https://www.instagram.com/eurobodallacouncil)

