

Moruya Airport – Aviation Based Development

If you are considering leasing land and building a hangar or other aviation based development at the Moruya Airport, you will need to lodge a development application (DA).

The Airport Masterplan and Precinct zoning are designed to make development of aviation related development as simple as possible. This Fact Sheet provides you with a guide to get you through the process.

<p>Step 1 – Discussions with Property Unit.</p>	<p>Discuss your requirements, possible lease location and proposed use with Property Unit.</p> <p>Generally, Council’s permitted use for land leased at Moruya Airport is “Aviation related commercial operations”.</p>
<p>Step 2 – confirm Airport operational requirements.</p>	<p>Airside</p> <p>When designing areas, it is prudent to provide space for itinerant aircraft.</p> <p>In this development aircraft cannot be parked on the airport side of the fence as it infringes on the taxiway clearance. Parking areas need to be provided for within the leased area.</p> <p>Taxiway Golf – code B. (see diagram below) The taxiway strip width is 20m from centreline to the fence. This is for aircraft wingspans of 15m up to 24m.</p> <p>Building heights are limited to remain below the Obstacle Limitation Surface (OLS). The diagram shows the slope of the OLS. It is recommended to include a roof clearance buffer to allow for future aerals etc.</p> <p>Please discuss any operational requirements for your use i.e where visiting aircraft can park, or taxiway suitability with the Airport staff. Airport Coordinator: 0409 037 520</p>
<p>Step 3 – Does your development need consent?</p>	<p>Most types of land uses and building to be established at the airport will need approval from Council. This means that you will need to lodge a development application (DA) and obtain land owners consent from Council for the proposed building.</p> <p>Council’s Property Unit of Council can assist you with any land ownership and leasing matters. Property Unit: 4474 1330</p> <p>Council’s Development Services Unit can assist you with the DA process. Please call 4474 1231.</p>
<p>Step 4 – Research & pre-application consultation</p>	<ul style="list-style-type: none"> • Zone: The Airport has a Special Activities zoning (SP1) under Council’s Local Environmental Plan (LEP) Controls. This means that only aviation related development or development ancillary to the airport can occur within the Airport grounds. • Setbacks: <ul style="list-style-type: none"> ○ There are no specific planning controls in terms of setbacks, site coverage or landscaping controls.

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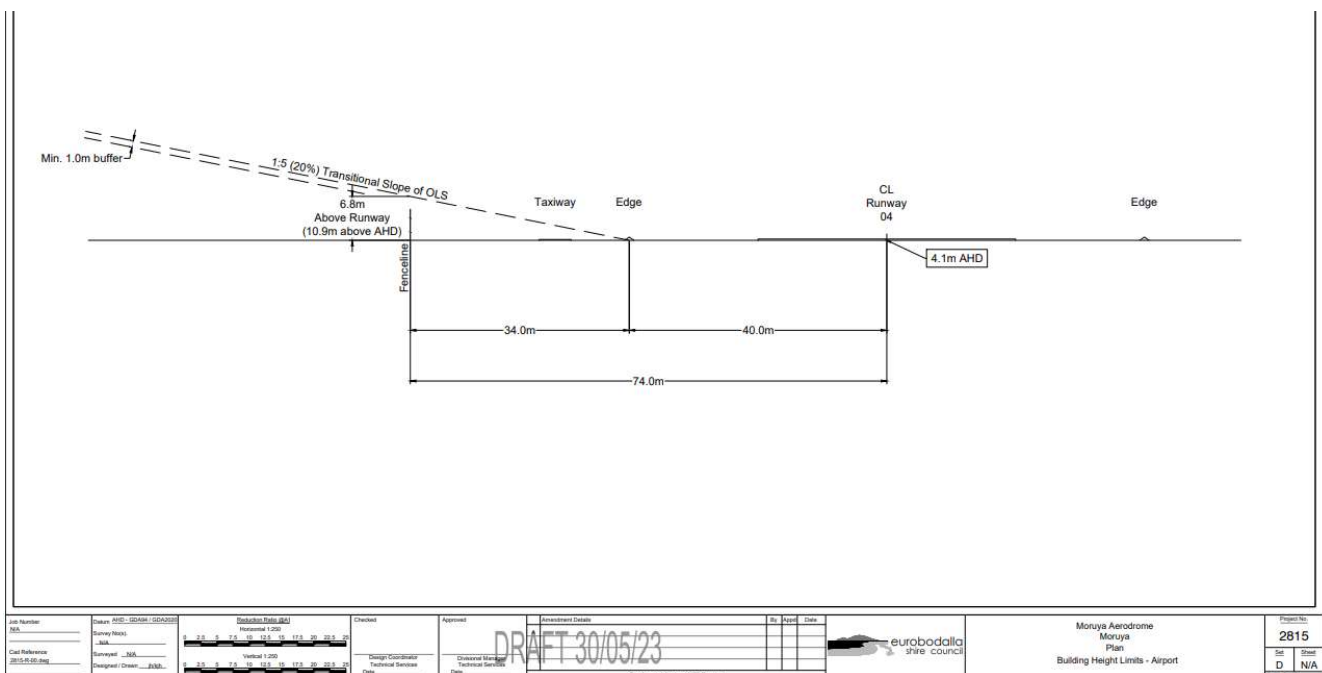
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	<ul style="list-style-type: none"> ○ Council recommends taking guidance from the Industrial Zones DCP. ○ Achieve compliance with the Building Code of Australia (BCA). Generally, structures are setback 3 metres from boundaries for fire safety reasons. You can go closer to the boundary, but you will need to use fire resistant construction. ○ Accessing the hangar from the airport may require taxiways/aprons that may require drainage and works within the airport grounds. Discuss these requirements with airport staff prior to DA application. ○ Compliance with CASA guidelines (wind shear, colours, etc) ● Sewer: the site is serviced by pressurised sewer mains. A pressure sewer pump will need to be proposed with any development. The sewer pump will need to be in such a location that access can be provided by Council staff should an issue arise in the future. A conceptual sewer plan is required to be lodged with the application. ● Water: the site is serviced by mains water. Rainwater use is also encouraged. If required, a metered water connection, lodged with Council works department, will be needed to access the mains water (connection fee applies). ● Stormwater: A conceptual stormwater plan is required to be provided including an on-site detention. ● Bushfire: The land is bushfire prone land, and any development must consider Planning for Bushfire Protection 2019 and relevant legislation. This is of particular importance when proposing tourist and visitor accommodation and or managers residences, early advice should be sought from the NSW RFS. Depending on the size of the building, you may also need to consider water storage for firefighting purposes. ● Generally, telecommunications and electricity supply will be available to the land but you should consult with Telstra and Essential Energy. ● As your development is on public land (owned by Council), you will need to enter into a lease.
<p>Step 5 – Preparing and lodging your DA</p>	<ul style="list-style-type: none"> ● See councils Development application (DA) Process ● DA must be lodged via the NSW planning portal with all required supporting documentation. You will also need to upload the relevant checklists, see above link. ● A DA will need to be accompanied by plans of the structure or building proposed including site plan, floor plan and elevations. The plans must be to scale and dimensioned. ● A Statement of Environmental Effects (SEE) is also required. This document provides a written description of your proposal including the nature of the business, work hours, staff numbers, service vehicles and environmental impacts such as noise and waste. ● As the building is proposed within Airport grounds, airport safety and the safety of aircraft utilizing the facility is essential. Depending on your proposal you may need to address additional matters such as light spillage, reflectivity of building material and to avoid bird attraction.
<p>Step 6 – Assessment of your application</p>	<p>A Development Assessment Officer will assess your application under 4.15 of the Environmental Planning and Assessment Act 1979. This involves inspecting the site and considering all relevant planning controls and Council policies, the likely impacts of the development, the suitability of the site for the proposed development.</p> <p>Some of the matters considered include:</p> <ul style="list-style-type: none"> ● Traffic parking; ● Height, form and materials; ● Building setbacks;

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	<ul style="list-style-type: none"> • Visual and acoustic privacy; • Servicing of the development including water supply, sewerage disposal and stormwater runoff. • Windsheer effect of building design • Bushfire protection • Compliance with aviation authority guidelines.
<p>Step 7 – Determination & notice</p>	<p>Your assessor will then make a determination granting conditional approval or refusal of the DA. You will receive a notice advising of the determination and where approval is granted, this notice will include conditions of consent and stamped plans.</p>
<p>Step 8 – Construction certificate</p>	<p>After you receive development consent, and before building work can commence, you will need to obtain a Construction Certificate (designed to ensure your proposal will meet the required standards i.e. the Building Code of Australia & the development consent). Once a Construction Certificate has been issued, building works can commence.</p>
<p>Step 9 – Commencement</p>	<p>You will need to notify Council of the proposed commencement date and confirm the appointment of the Principal Certifier (PC) for the development. This is usually the same person or organisation that issues the Construction Certificate. The PC is required to carry out inspections at specific stages of the works and to issue an Occupation Certificate upon completion of the development.</p>



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Ongoing Costs

There are initial costs and ongoing annual costs of building on airport land.

These costs vary depending on the size of the block, the services to site, the type of use of the block and the level of complication and detail of the application.

A typical summary of costs is shown below for awareness.

Initial Establishment costs

1. NSW Land Registry Services Lease registration fees of \$165.40 inc. GST.
2. PEXA fee of \$68.86 inc. GST.
3. Legal fees estimated amount of \$550.00.
4. 3-month rent security bond

Annual costs to Council

Rate charges are calculated on the site use, land area, services to site, and waste management services in place.

Typical rates (2023) for these are:

Lease payment	- \$10000
Environmental	- \$27.03
Standard business	- \$1198.10
Water supply	- \$600.00
Sewer supply	- \$1728.00
Waste management	- \$58.80
Total	- \$13611.97

*The figures shown are indicative of a medium sized block and a non-production facility. (Actual rates will vary.)
Third-party costs eg. Electricity, Insurance and Telecommunications are additional.*

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Airspace planning considerations

Objective:

- Maintain the viability and safety of the airport by protecting its airspace, operational and security areas. Requirements:

a) Buildings or structures must not create an obstruction or potential hazard, by way of attracting birds, emitting bright lights, smoke, or any other activity or air emissions hazardous to aircraft movements.

b) The effect on occupants of buildings or structures within the GA precinct resulting from the movement of aircraft needs to be considered in development applications.

c) Developments shall be designed to not impact on the Obstacle Limitation Surfaces. This includes all building components, including roof mounted structures such as air conditioning units, antennas and exhaust systems.

Built form

Building setbacks and articulation

Objective:

- Ensure all buildings in the GA Precinct contribute positively to the built environment and blend into the surrounding bushland and coastal environment.

Requirements:

a) All buildings must be set back at least 1.5m from the side boundaries of lease areas or, where lease areas are consolidated, 1.5m from the side boundary of the consolidated lease area.

b) Where building separation is required to be more than the shared separation distance of 3m, side setbacks shall be increased so that the adjacent lease area/s are not restricted by the development.

c) Office premises, accommodation and public-facing parts of developments are to utilise greater articulation within facades including generous use of glazing. Articulation can be achieved through building setbacks or projecting building elements, and by varying roof forms, building materials, finishes and colours.

d) Main entrances are to be clearly articulated and visible from the GA precinct road.

e) Entrance points to the buildings are to be designed as focus points and must provide protection for pedestrians with integrated verandas, canopies or colonnades.

f) Buildings must be bird proofed by incorporating measures such as minimising ledges and perches for birds to congregate and enclosing eaves to reduce bird roosting.

Roof forms

Objective:

- Ensure aircraft can access hangars.
- Integrate roof forms into the overall design of the building.

Requirements:

a) Utilise varied roof forms to provide visual interest. Avoid flat, bulky or highly detailed roof forms.

b) Roof forms should be designed to complement existing roof forms in the GA precinct.

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- c) Utilise roof forms to differentiate between the various elements of the building.
- d) All plant and equipment and any other potentially visually intrusive elements are to be concealed within roof forms to minimise visual impact.
- e) Structures attached to roofs must remain below the clearance buffer of the Obstacle Limitation Surface

Car parking and loading docks

Objective:

- Provide sufficient parking on site to meet the needs of businesses, visitors and any associated uses, such as deliveries or warehouse handling and manoeuvring.

Requirements:

- a) All parking requirements must be integrated on individual lease areas outside or within buildings, or on airport grounds in agreement with the airport owner and in accordance with a lease or licensing arrangement.
- b) Vehicle crossovers must be constructed at the developer's expense.
- c) All parking and manoeuvring spaces shall be engineered to ensure adequate strength and durability against rain, wind and wear & tear.
- d) Loading docks must be of an appropriate size to adequately service on site operations without overspill to roadways.

Visual amenity

Colours, materials and finishes

Objectives:

- Establish a cohesive and consistent design character by using a co-ordinated palette of colours, materials and finishes.
- Ensure that building design does not impact airport operations.

Requirements:

- a) Utilise materials that reinforce the bushland and coastal built form and landscape such as corrugated iron, unpainted timber and textured concrete. Avoid excessive use of heavy looking materials and unfinished pre-cast concrete walls.
- b) Large building components such as sheds or hangars shall provide articulation and utilise a mix of building materials, colours and textures to provide visual interest and reduce bulkiness.
- c) The use of corporate colours on buildings is limited to approved signage.
- d) All roofs must be non reflective.
- e) Main wall elevations, gable treatments, gutters and other building elements may be any combination of colours to compliment the landscape and not provide a visual distraction to aviation.

Landscaping

Objectives:

- Soften the appearance of buildings, fencing, carparking and hardstand areas by integrating landscaping into the design.

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- Ensure appropriate siting of landscaping elements with consideration of airport operations and security and proximity to underground services.

- Enhance air quality and reduce the heat retaining effect of hard surfaces.

Requirements:

- a) Development applications must be supported with a landscape plan.
- b) The location and type of planting within landscaping must not impede the function of, or access to, services (including underground infrastructure), facilities and sight lines for vehicle movements and airport operations, or impact the Obstacle Limitation Surfaces when mature. In some circumstances, root guards may be required for larger plants.
- c) Landscaping selection is limited to vegetation that does not attract birds. As a guide, the following species are acceptable: Botanical name Common name Carex apressa Tall sedge, Ficinnia nodosa Knobbly club rush, Juncus usitatus Common rush, Juncus krausii Sea rush, Lomandra glauca, Pale mat-rush, Lomandra longifolia 'Tanika' 'Tanika' mat-rush, Lomandra longifolia Common mat-rush, Poa labillardierei Common tussock grass, Grevillea 'Royal Mantle' Grevillea groundcover, Tristaniopsis laurina Water gum, Syzygium australe 'Hinterland gold' Gold lilly pilly, Elaeocarpus reticulatis Blueberry ash.
- d) Planting and landscape design shall reinforce entries and parking areas.
- e) For security reasons, planting must be at least 1.5m clear of the airside/landside boundary.
- f) Landscaping or screening must be provided around car park areas and loading docks and, to provide visual relief from long building facades.
- g) Any portion of a development site that remains unused or vacant following development must be landscaped or turfed and maintained in a tidy condition.

Signage

Objectives:

- Ensure signage and advertising is designed and located to avoid visual clutter.
- Provide appropriate visual exposure for businesses and wayfinding for visitors.

Requirements:

- a) Single occupant leased areas are permitted 1 freestanding sign not exceeding 2.5sqm within the landscaped area of the site with a maximum height of 5m above existing ground level and 1 façade sign, not exceeding 5sqm, located no higher than the parapet or eaves of the building to which it is attached.
- b) Signage must be attached to the building in which the business identified in the sign is located.
- c) For multi-tenancy sites, a directory board located near the entry to the development or within the landscaped area of the site is permitted, indicating the product or service offered and each occupant of the site. In addition, 1 façade sign at the roadside and another at the airside is permitted per tenancy, not exceeding 5sqm, of uniform size, shape and construction, positioned within the bounds of the wall surface, no higher than the parapet or eaves of the building to which it is attached.

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d) The use of corporate colours is limited to the nominated signage space of the development.

e) The following is not permitted:

- signage attached to front fences
- A-Frame signage within the road reserve
- roof signs
- signs that cover any window, door or architectural feature
- projecting signs
- flashing or illuminated signs
- signs with information that does not relate to the site or its tenants
- signs which rotate or move
- sequins and glitter.

Lighting

Objective:

- Provide sufficient lighting for the privately leased areas of the GA Precinct without compromising airfield operations.

Requirements:

a) Lighting must be provided to the external entry path, building entryway/s, common lobby, driveway, and car parking using vandal resistant, high mounted light fixtures.

b) Development must avoid the creation of a lighting hazard through the appropriate design, lighting colour, position, pattern and intensity of lighting.

c) All lighting must be consistent with the requirements of NASF Guideline E. [GuidelineE.pdf \(infrastructure.gov.au\)](#)

d) No external lighting shall be directed beyond the lease boundary.

e) High efficiency or more sustainable lighting sources should be utilised where appropriate.

Timer controls or motion sensors are encouraged both internally and externally.

Fencing

Objective:

- Provide adequate security for airport operations and businesses in the GA Precinct whilst promoting the cohesive visual appeal of the streetscape.

Requirements:

a) Security fencing is to be installed and maintained to all airside interfaces within the lease area.

b) Razor wire (or the like) fencing is not permitted.

d) Security fencing must be of high-quality materials, integrated with walling and, include appropriate low-scale landscaping to reduce visual impact.

External service, wash down bays, rubbish and storage areas

Objectives:

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- Minimise the potential for the activities within the GA Precinct to attract birds or vermin or impact visual amenity.

- Ensure compliant stormwater services infrastructure.

Requirements:

a) All developments must carry out the necessary works to connect to existing sewer, water, stormwater, communications and electricity services.

b) New development will be designed so that stormwater flows are managed in accordance with the Building Code of Australia, Council's stormwater management guidelines and the GA precinct's stormwater infrastructure design. On-site stormwater detention facilities and re-use of stormwater on site may be required.

c) All developments must provide an appropriate waste storage area.

d) Waste and recycling systems are to be healthy, safe and easy to use and, where necessary, collection vehicles are able to safely access buildings or sites to remove waste and other resources.

e) Rubbish bin storage areas must not be on the airside frontage. Where located on the street-side they shall be enclosed and structurally screened.

f) All services such as water storage tanks shall be integrated with the building design.

g) Wash down areas shall be connected to the sewer system.

h) No contaminated water is to be allowed into the storm water drain.

i) The front setback shall not be used for the storage of any goods or materials or be used for any use other than its approved use.

Energy efficiency

Objective:

- Achieve energy efficient and environmentally sensitive development.

Requirements:

a) The design of buildings will have regard to siting, layout and construction methods that minimise the need for fossil fuel based heating, cooling and lighting. The use of water saving features and energy efficient heating systems is encouraged.

Aboriginal archaeology

Objective:

- Ensure the impact of a proposed development on the heritage significance of an Aboriginal place, cultural value, landscape value or object is considered by adequate investigation and assessment processes.

Requirements:

a) All development must comply with the requirements of Aboriginal Heritage Impact Permit

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